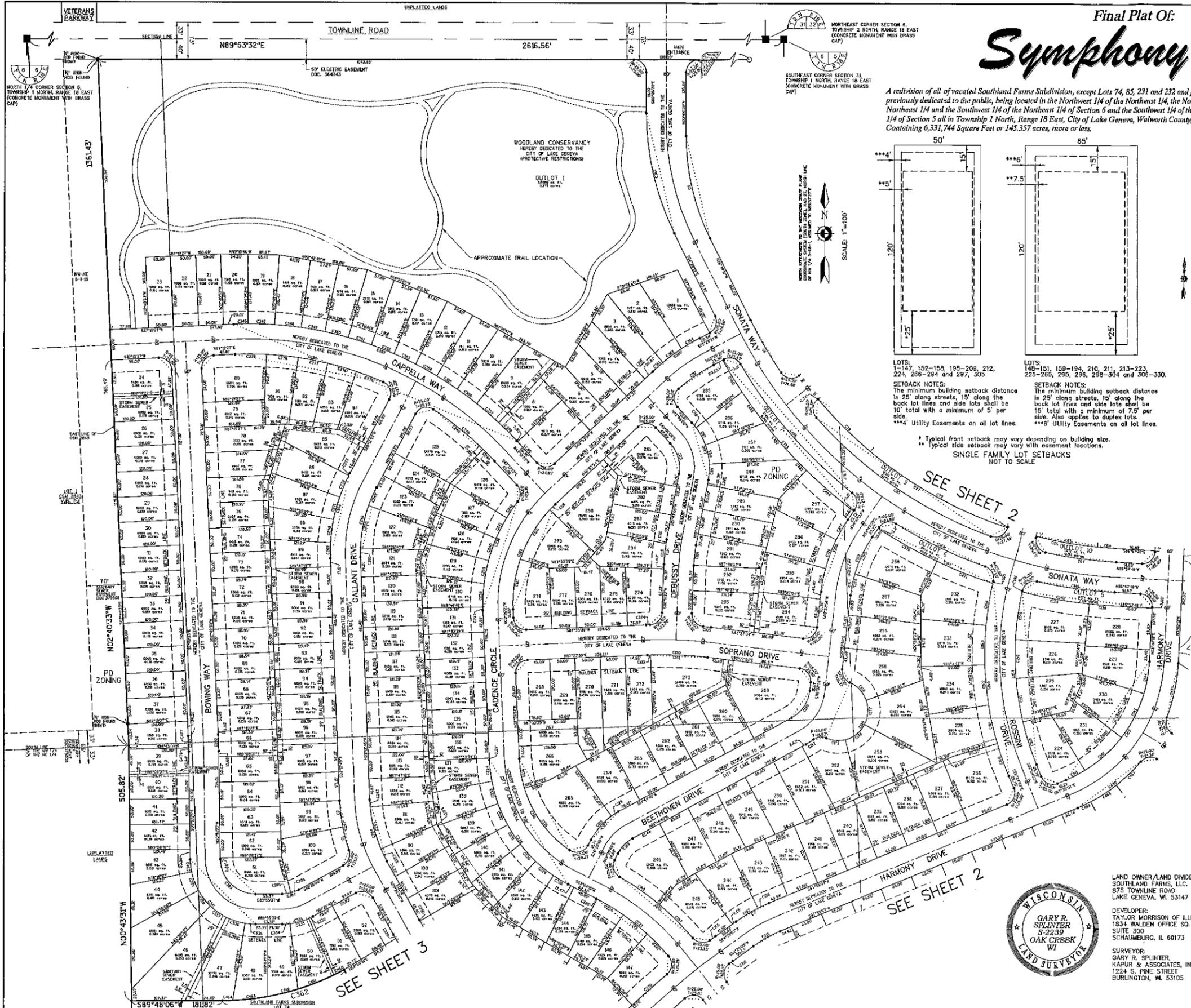
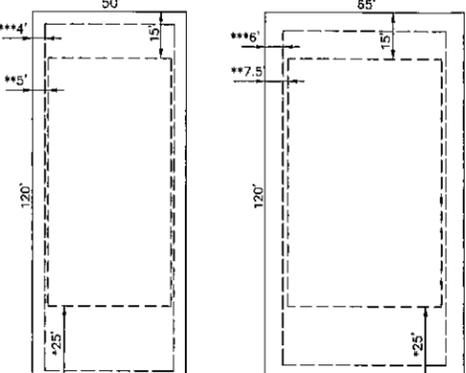


**FINAL
PLAT**



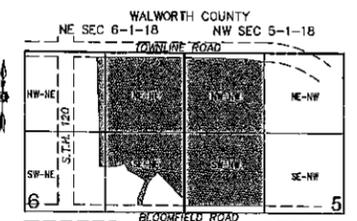
Final Plat Of:
Symphony Bay

A revision of all of vacated Southland Farms Subdivision, except Lots 74, 85, 231 and 232 and portions of Townline Road and Bloomfield Road previously dedicated to the public, being located in the Northwest 1/4 of the Northeast 1/4, the Northeast 1/4 of the Northeast 1/4, the Southeast 1/4 of the Northeast 1/4 and the Southwest 1/4 of Section 5 and the Southwest 1/4 of the Northwest 1/4 and the Northwest 1/4 of the Northwest 1/4 of Section 5 all in Township 1 North, Range 18 East, City of Lake Geneva, Walworth County, State of Wisconsin. Containing 6,331,744 Square Feet or 145.357 acres, more or less.



LOTS: 1-147, 152-159, 195-206, 212, 224, 256-294 and 297, 305
 SETBACK NOTES: The minimum building setback distance is 25' along streets, 15' along the back lot lines and side lots shall be 10' total with a minimum of 5' per side. ***4' Utility Easements on all lot lines.

LOTS: 148-151, 159-184, 210, 211, 213-223, 225-265, 295, 296, 298-304 and 306-350.
 SETBACK NOTES: The minimum building setback distance is 25' along streets, 15' along the back lot lines and side lots shall be 15' total with a minimum of 7.5' per side. Also applies to duplex lots. ***6' Utility Easements on all lot lines.



NOTES:
 1) SYMPHONY BAY IS A PLANNED DEVELOPMENT.
 2) AS PER AN AFFIDAVIT OF CORRECTION (DOC. #753573) TO THE FINAL PLAT OF SOUTHLAND FARMS, ACCESS TO EASEMENT IS TO BE PROVIDED BY DEVELOPMENT SOLUTIONS AND NEXTEL WEST CORPORATION.
 3) IN LIEU OF SETTING MONUMENTS FOR A REASONABLE TIME, A SURETY BOND AS PER W STATUTES 236.15(10) SHALL BE EXECUTED WITH THE CITY OF LAKE GENEVA.

- LEGEND:
- 2.375" O.D. X 2"(MINIMUM) IRON PIPE 3.65 LBS./LIN. FT. SET
 - 1 1/4" O.D. IRON PIPE FOUND OR 3/4" O.D. IRON ROD FOUND AS INDICATED
 - ALL OTHER LOT CORNERS 1 1/4" O.D. X 18"(MINIMUM) IRON PIPE 1.95 LBS./LIN SET
 - WETLANDS DELINEATED OCTOBER, 2015 KAPUR & ASSOCIATES, INC.
 - BUILDING SETBACK LINE
 - SECTION CORNER
 - OTIP = DEDICATED TO THE PUBLIC
 - O.D. = OUTSIDE DIAMETER
 - R.A. = RECORDED AS
 - T = TANGENT
 - R = RADIUS
- ALL LINEAR MEASUREMENTS HAVE BEEN MADE TO THE NEAREST 0.01 OF A FOOT. ALL DISTANCES SHOWN ALONG CURVED LINES ARE ARC DISTANCES.



LAND OWNER/LAND DIVIDER:
 SOUTHLAND FARMS, LLC.
 875 TOWNLINE ROAD
 LAKE GENEVA, WI 53147

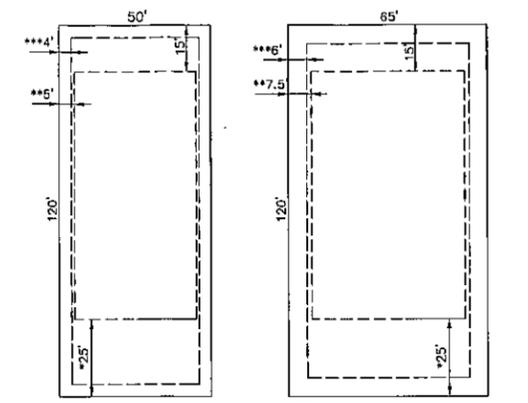
DEVELOPER:
 TAYLOR MORRISON OF ILLINOIS, INC.
 1834 WALDEN OFFICE SQ.
 SUITE 300
 SCHUMBERG, IL 60173

SURVEYOR:
 GARY R. SPLINTER
 KAPUR & ASSOCIATES, INC.
 1224 S. FINE STREET
 BURLINGTON, WI 53105

KAPUR & ASSOCIATES, INC.
 CONSULTING ENGINEERS
 1224 SOUTH FINE STREET
 BURLINGTON, WISCONSIN 53106
 Phone: 262-787-2747 Fax: 262-787-2799
 www.kapurengineers.com

Final Plat Of: Symphony Bay

A redivision of all of vacated Southland Farms Subdivision, except Lots 74, 85, 231 and 232 and portions of Townline Road and Bloomfield Road previously dedicated to the public, being located in the Northwest 1/4 of the Northeast 1/4, the Northeast 1/4 of the Northeast 1/4, the Southeast 1/4 of the Northeast 1/4 and the Southwest 1/4 of the Northeast 1/4 of Section 6 and the Southwest 1/4 of the Northwest 1/4 and the Northwest 1/4 of the Northwest 1/4 of Section 5 all in Township 1 North, Range 18 East, City of Lake Geneva, Walworth County, State of Wisconsin.
Containing 6,331,744 Square Feet or 145.357 acres, more or less.

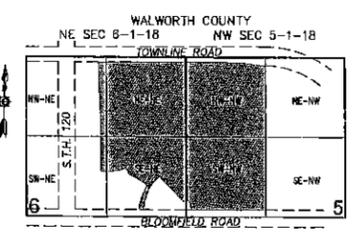
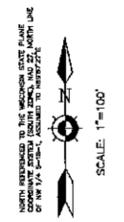


LOTS:
1-147, 152-158, 195-205, 212, 224, 266-294 and 297, 305

SETRBACK NOTES:
The minimum building setback distance is 25' along streets, 15' along the back lot lines and side lots shall be 10' total with a minimum of 7.5' per side. Also applies to duplex lots.
***6' Utility Easements on all lot lines.

* Typical front setback may vary depending on building size.
** Typical side setback may vary with easement locations.

SINGLE FAMILY LOT SETBACKS
NOT TO SCALE



NOTES:
1) SYMPHONY BAY IS A PLANNED DEVELOPMENT.
2) AS PER AN AFFIDAVIT OF CORRECTION (DOC. #753573) TO THE FINAL PLAT OF SOUTHLAND FARMS, ACCESS TO EASEMENT IS TO BE PROVIDED BY DEVELOPMENT SOLUTIONS AND NEXTEL WEST CORPORATION.
3) IN LIEU OF SETTING MONUMENTS FOR A REASONABLE TIME, A SURETY BOND AS PER WIS. STATUTES 236.15(4) SHALL BE EXECUTED WITH THE CITY OF LAKE GENEVA.



LAND OWNER/LAND DIVIDER:
SOUTHLAND FARMS, LLC.
875 TOWNLINE ROAD
LAKE GENEVA, WI. 53147

DEVELOPER:
TAYLOR MORRISON OF ILLINOIS, INC.
1834 WALDEN OFFICE SQ.
SUITE 300
SCHAUMBURG, IL 60173

SURVEYOR:
GARY R. SPLINTER,
KAPUR & ASSOCIATES, INC.
1224 S. PINE STREET
BURLINGTON, WI. 53105

LEGEND:
• 2.375" O.D. X 24" (MINIMUM) IRON PIPE 3.65 LBS./LIN. FT. SET
• 1 1/4" O.D. IRON PIPE FOUND OR 3/4" O.D. IRON ROD FOUND AS INDICATED
ALL OTHER LOT CORNERS 1 1/4" O.D. X 18" (MINIMUM) IRON PIPE 1.65 LBS./LIN. SET

WETLANDS DELINEATED OCTOBER, 2015 KAPUR & ASSOCIATES, INC.
BUILDING SETBACK LINE
SECTION CORNER
D.T.P. = DEDICATED TO THE PUBLIC
O.D. = OUTSIDE DIAMETER
R.A. = RECORDED AS
T = TANGENT
R = RADIUS

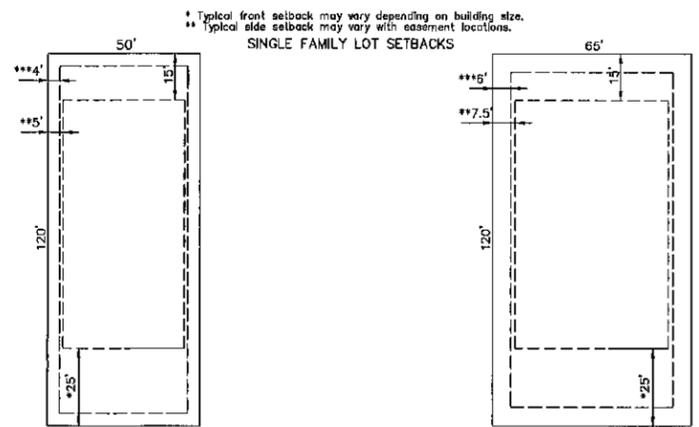
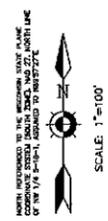
ALL LINEAR MEASUREMENTS HAVE BEEN MADE TO THE NEAREST 0.01 OF A FOOT.
ALL DISTANCES SHOWN ALONG CURVED LINES ARE ARC DISTANCES.



KAPUR & ASSOCIATES, INC.
CONSULTING ENGINEERS
1224 SOUTH PINE STREET
BURLINGTON, WISCONSIN 53105
Phone: 262-767-2747 Fax: 262-767-2769
www.kapurengineers.com

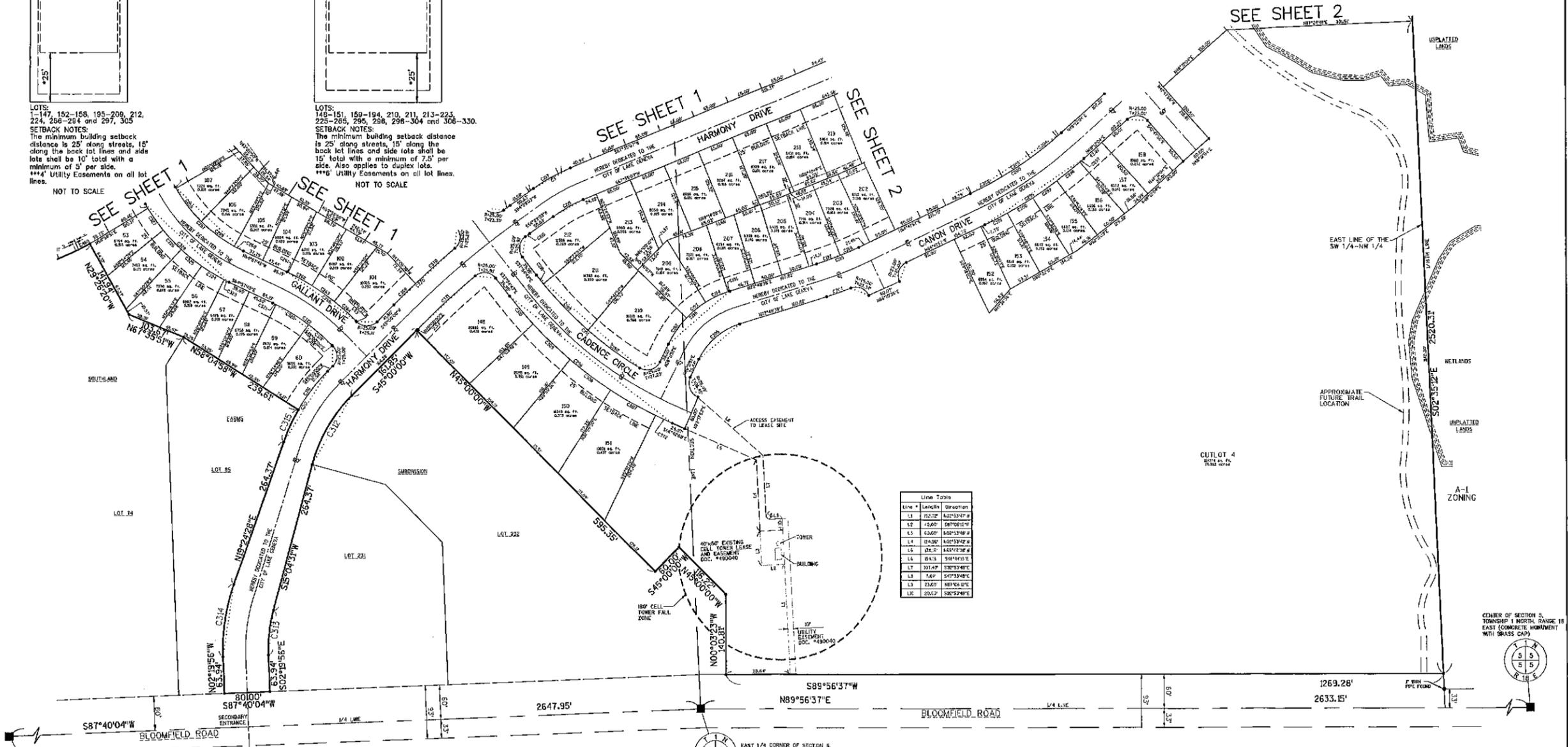
Final Plat Of: Symphony Bay

A redivision of all of vacated Southland Farms Subdivision, except Lots 74, 85, 231 and 232 and portions of Townline Road and Bloomfield Road previously dedicated to the public, being located in the Northwest 1/4 of the Northeast 1/4, the Northeast 1/4 of the Northeast 1/4, the Southeast 1/4 of the Northeast 1/4 and the Southwest 1/4 of the Northeast 1/4 of Section 6 and the Southwest 1/4 of the Northwest 1/4 and the Northwest 1/4 of the Northwest 1/4 of Section 5 all in Township 1 North, Range 18 East, City of Lake Geneva, Walworth County, State of Wisconsin. Containing 6,331,744 Square Feet or 145.337 acres, more or less.



LOTS: 1-147, 152-168, 195-206, 212, 224, 256-264 and 297, 305
SETBACK NOTES: The minimum building setback distance is 25' along streets, 15' along the back lot lines and side lots shall be 15' total with a minimum of 5' per side. ***4' Utility Easements on all lot lines.

LOTS: 148-151, 159-194, 210, 211, 213-223, 225-265, 295, 298, 299-304 and 308-330.
SETBACK NOTES: The minimum building setback distance is 25' along streets, 15' along the back lot lines and side lots shall be 15' total with a minimum of 7.5' per side. Also applies to duplex lots. ***6' Utility Easements on all lot lines.



Line #	Length	Direction
L1	152.72'	S20°54'47" E
L2	13.60'	S67°08'15" E
L3	63.60'	S60°33'48" E
L4	84.30'	S20°52'42" E
L5	185.75'	S42°42'29" E
L6	84.13'	S81°43'12" E
L7	101.47'	S30°33'48" E
L8	1.60'	S07°33'48" E
L9	73.60'	N81°54'15" E
L10	20.67'	S82°52'48" E



CENTER OF SECTION 6, TOWNSHIP 1 NORTH, RANGE 18 EAST (CONCRETE MONUMENT WITH BRASS CAP)

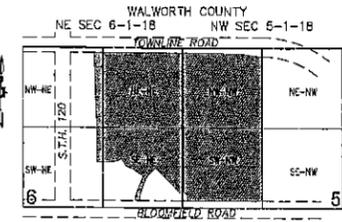
NOTES:
1) SYMPHONY BAY IS A PLANNED DEVELOPMENT.
2) AS PER AN AFFIDAVIT OF CORRECTION (DOC. #753573) TO THE FINAL PLAT OF SOUTHLAND FARMS, ACCESS TO EASEMENT IS TO BE PROVIDED BY DEVELOPMENT SOLUTIONS AND NEXTEL WEST CORPORATION.
3) IN LIEU OF SETTING MONUMENTS FOR A REASONABLE TIME, A SURETY BOND AS PER WIS. STATUTES 236.15(1) SHALL BE EXECUTED WITH THE CITY OF LAKE GENEVA.



LAND OWNER/LAND DIVIDER:
SOUTHLAND FARMS, LLC.
875 TOWNLINE ROAD
LAKE GENEVA, WI. 53147

DEVELOPER:
TAYLOR MORRISON OF ILLINOIS, INC.
1834 WALDEN OFFICE SQ.
SUITE 300
SCHALMURG, IL. 60173

SURVEYOR:
GARY R. SPLINTER,
KAPUR & ASSOCIATES, INC.
1224 S. PINE STREET
BURLINGTON, WI. 53105



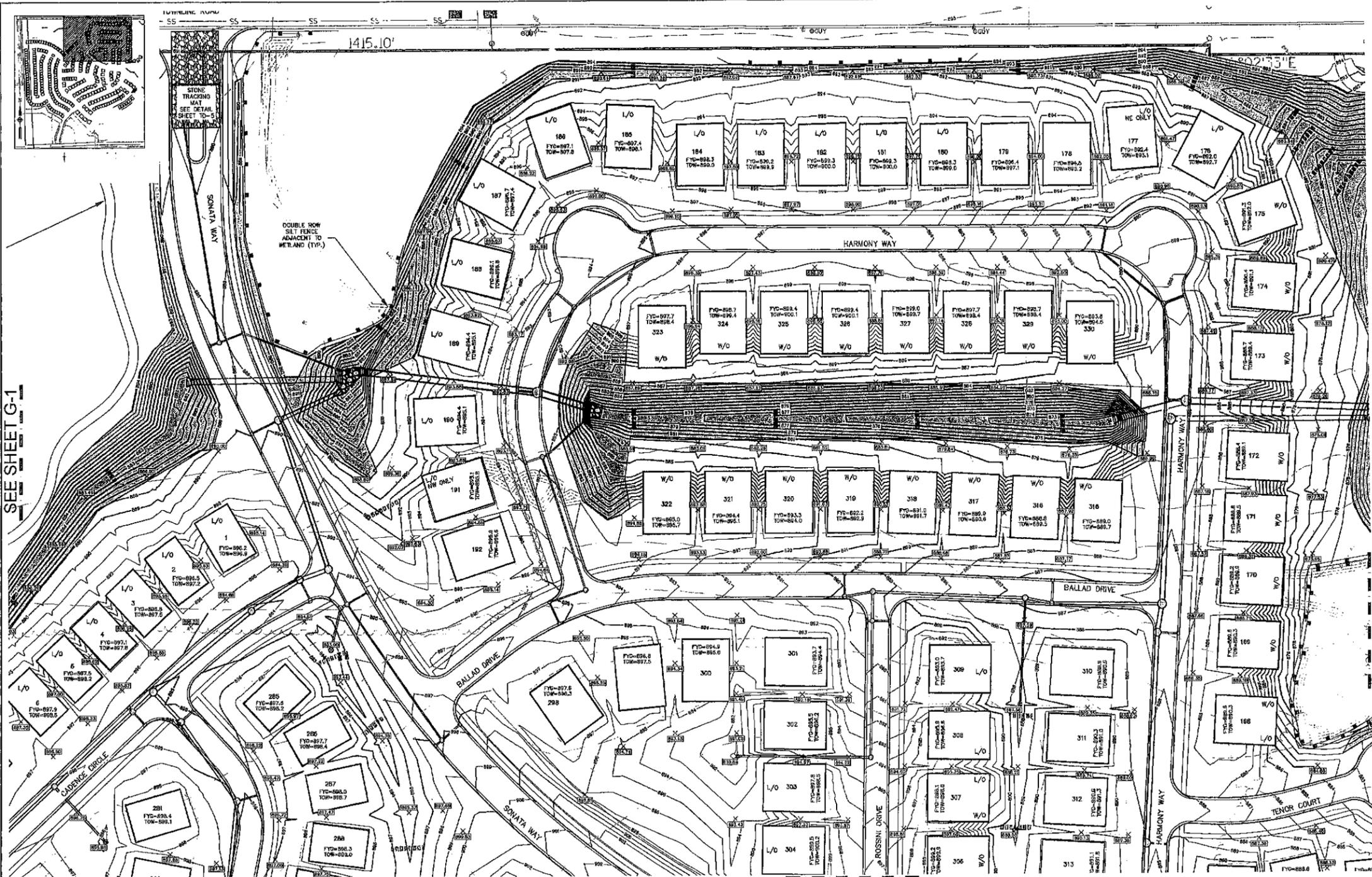
LOCATION SKETCH
NOT TO SCALE

- LEGEND:**
- 2.375" O.D. X 24"(MINIMUM) IRON PIPE 3.60 LBS./LN. FT. SET
 - 1 1/4" O.D. IRON PIPE FOUND OR 3/4" O.D. IRON ROD FOUND AS INDICATED
 - ALL OTHER LOT CORNERS 1 1/4" O.D. X 18"(MINIMUM) IRON PIPE 1.65 LBS./LN. SET
 - WETLANDS DELINEATED OCTOBER 2015 KAPUR & ASSOCIATES, INC.
 - BUILDING SETBACK LINE
 - SECTION CORNER
 - DTTP = DEDICATED TO THE PUBLIC
 - O.D. = OUTSIDE DIAMETER
 - R.A. = RECORDED AS
 - T = TANGENT
 - R = RADIUS



KAPUR & ASSOCIATES, INC.
CONSULTING ENGINEERS
1224 SOUTH PINE STREET
BURLINGTON, WISCONSIN 53105
Phone: 262-767-2747 Fax: 262-767-2768
www.kapurengineers.com

**FINAL
ENGINEERING**



SEE SHEET G-1

SEE SHEET G-3

SEE SHEET G-5

DIGGERS HOTLINE
 Toll Free (800) 242-8511
 Milwaukee Area (414) 288-1181
 Hearing Impaired TDD (800) 542-2288
 www.DiggersHotline.com

RESPECT ALL EROSION CONTROL MEASURES PRIOR TO COMMENCING GRADING. GRADING OR OTHER LAND DISTURBING ACTIVITIES. EROSION CONTROL MEASURES MUST BE INSTALLED NEARLY AND WITHIN 24 HOURS OF EXISTING PRECIPITATION EVENT OF 0.50 INCH OR GREATER. IN ADDITION THE CONTRACTOR SHALL CONDUCT DAILY INSPECTIONS AND DOCUMENT CONDITIONS AND REPAIRS. ALONG WITH DATE, TIME OF INSPECTION AND WEATHER CONDITIONS IN A DAILY LOG BOOK. THE DAILY LOG BOOK, MEETS 7.2 AND MUST BE SUBMITTED TO THE CITY ENGINEER AND WRITER PERMIT SHALL BE KEPT IN AN ACCESSIBLE LOCATION FOR A MINIMUM OF 90 DAYS AFTER THE WORK IS COMPLETED.

AT ABSOLUTELY NO TIME MAY CONSTRUCTION EQUIPMENT, DEBRIS, FILL, ETC. BE PLACED NEAR NEIGHBORS, WATERWAYS OR FLOOD PLAINS UNLESS IDENTIFIED IN THE PLANS.

1. ALL RECOMMENDED GRADERS ARE TO FINISH TO GRADE.
2. FINISHED FLOOR ELEVATIONS: FFC = FFC17.0 - 17.000
3. TOP OF FOUNDATION WALL ELEVATIONS: FFW = AS SHOWN
4. GRADERS ARE SUBJECT TO CHANGE DEPENDING ON LOCATION OF HOUSE AND JOB CONDITIONS UPON ENGINEER'S APPROVAL.
5. SOIL EROSION CONTROL SHALL BE CONSTRUCTED AT THE TIME OF GROUND PLACEMENT.
6. ALL EXISTING SITE FIRM BOUNDARY LINES WHICH ARE ENCOUNTERED DURING CONSTRUCTION OPERATIONS SHALL BE REPAIRED, REPAIRED AND/OR CORRECTED TO THE PROPOSED SYSTEM OVER SYSTEMS. THE CONTRACTOR SHALL MAINTAIN DOCUMENT THE LOCATION AND REPAIR OF ALL EXISTING BOUNDARY LINES.
7. ALL EXISTING BOUNDARY LINES SHOULD BE LEFT LOWER THAN FINISHED GRADE AT THE CONSTRUCTION DISCRETION TO ALLOW FOR BOUNDARY LINES TO BE MAINTAINED THROUGH CONSTRUCTION.
8. ALL PROPOSED STORM CATCH BASINS TO BE PROTECTED PER DETAIL.

EROSION CONTROL LEGEND

- CLASS I, TYPE B EROSION MATTING
SEE DETAIL SHEET TD-4
- PYRAMAT OR EQUIVALENT CLASS II, TYPE D MATTING W/ 4" SUEDE TYPSON, SEED AND CLASS I, TYPE B EROSION MATTING
- MEDIUM DUTY RIPRAP
SEE DETAIL SHEET TD-5
- SILT SOCK DITCH CHECK, SEE DETAIL SHEET TD-4
- SILT FENCE, SEE DETAIL SHEET TD-5

GRADING LEGEND

- EXISTING CONTOUR MINOR
- EXISTING CONTOUR MAJOR
- PROPOSED CONTOUR MINOR
- PROPOSED CONTOUR MAJOR
- LOT CERT. ELEVATION
- PROPOSED FINISHED GRADE/
MIN. TOP OF FOUNDATION WALL
- LOOKOUT | WALKOUT

KAPUR & ASSOCIATES, INC.
 CONSULTING ENGINEERS
 1711 N. PORT WASHINGTON ROAD
 WEAUWATOSSE, WISCONSIN 53091
 Phone: 414.581.8188 Fax: 414.581.4117
 www.kapurengineers.com

PROJECT:
SYMPHONY BAY

LOCATION:
 CITY OF LAKE
 GENEVA, WI

CLIENT:

RELEASE:
 PRELIMINARY NOT
 FOR
 CONSTRUCTION

REVISIONS

#	DATE	DESCRIPTION
1	08/11/10	ISSUE
2	08/11/10	ISSUE
3	08/11/10	ISSUE
4	08/11/10	ISSUE
5	08/11/10	ISSUE
6	08/11/10	ISSUE
7	08/11/10	ISSUE
8	08/11/10	ISSUE
9	08/11/10	ISSUE
10	08/11/10	ISSUE



SCALE: 1" = 50'

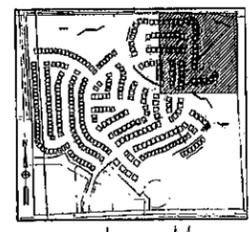
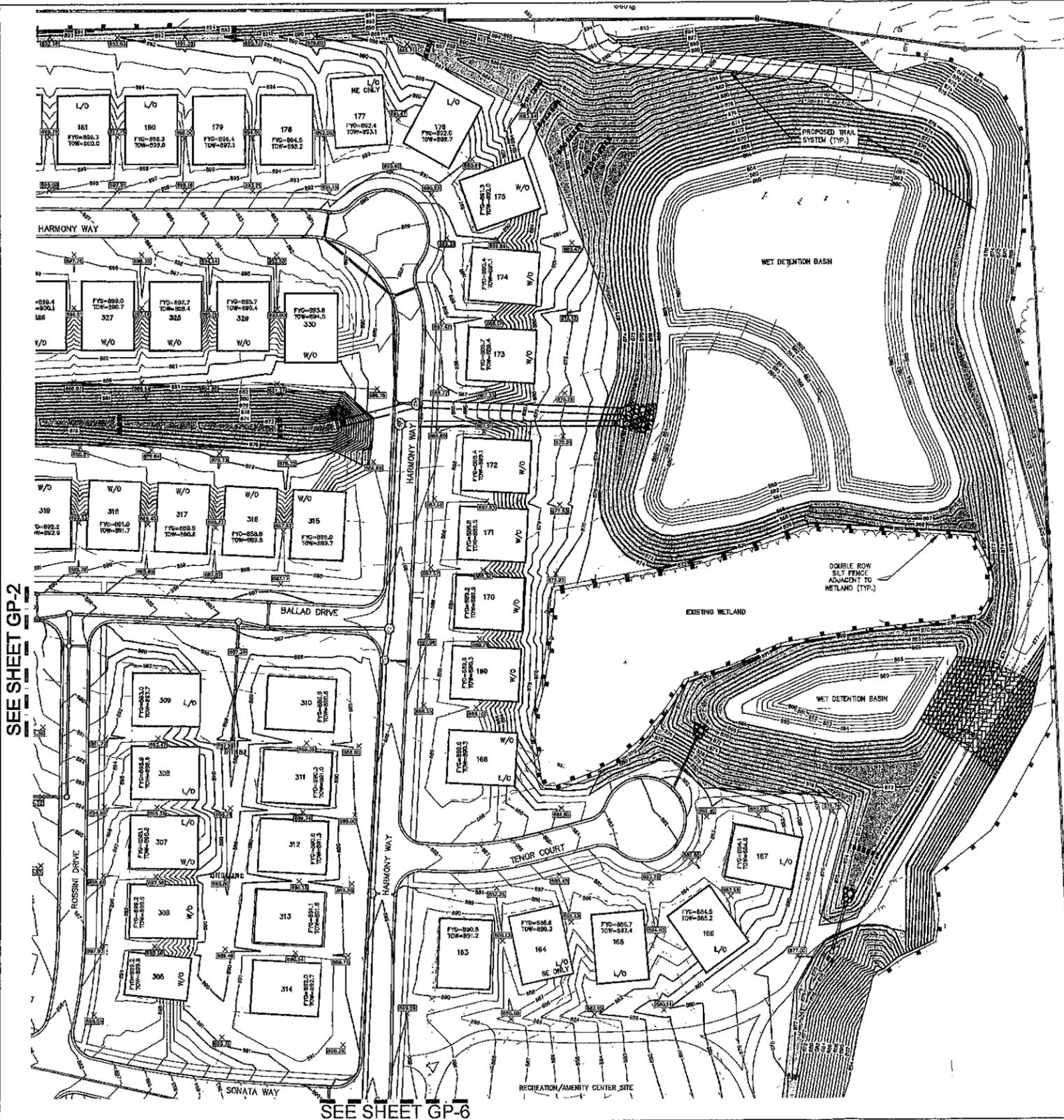
SEAL:

SHEET:
**GRADING AND
 EROSION CONTROL
 PLAN**

PROJECT MANAGER:
 PROJECT NUMBER: 15001
 DATE: 1-22-2010

SHEET NUMBER:
GP-2

FILENAME: D:\yhw\work\CA\lake Geneva_City\15001_Symphony Bay\Design\15001_GP_2_SHEET.dwg



KAPUR & ASSOCIATES, INC.
 CONSULTING ENGINEERS
 2711 N. FORT WASHINGTON ROAD
 MILWAUKEE, WISCONSIN 53217
 Phone: 414.358.0600 Fax: 414.358.4117
 www.kapurengineers.com

PROJECT:
SYMPHONY BAY

LOCATION:
CITY OF LAKE GENEVA, WI

CLIENT:

RELEASE:
PRELIMINARY NOT FOR CONSTRUCTION

REVISIONS:

#	DATE	DESCRIPTION
01	08/20/13	ISSUE FOR PERMIT
02	08/20/13	ISSUE FOR PERMIT
03	08/20/13	ISSUE FOR PERMIT
04	08/20/13	ISSUE FOR PERMIT
05	08/20/13	ISSUE FOR PERMIT
06	08/20/13	ISSUE FOR PERMIT
07	08/20/13	ISSUE FOR PERMIT
08	08/20/13	ISSUE FOR PERMIT
09	08/20/13	ISSUE FOR PERMIT
10	08/20/13	ISSUE FOR PERMIT



SCALE: 1" = 10'

DATE: 1/22/2018

SHEET:
GRADING AND EROSION CONTROL PLAN

PROJECT MANAGER:
 PROJECT NUMBER: 152021
 DATE: 1/22/2018

SHEET NUMBER:
GP-3

GRADING LEGEND

---	EXISTING CONTOUR MINOR
---	EXISTING CONTOUR MAJOR
---	PROPOSED CONTOUR MINOR
---	PROPOSED CONTOUR MAJOR
---	LOT CENT. ELEVATION
---	PROPOSED FINISHED GRADE / MIN. TOP OF FOUNDATION WALL
---	LOOKOUT WALKOUT

EROSION CONTROL LEGEND

[Symbol]	CLASS I, TYPE B EROSION MATTING SEE DETAIL SHEET TD-4
[Symbol]	PYRAMAT OR EQUIVALENT CLASS III, TYPE D MATTING W/ 4" MIN. TOPSOIL, SEED AND CLASS I, TYPE B EROSION MATTING
[Symbol]	MEDIUM DUTY RIPRAP SEE DETAIL SHEET TD-5
[Symbol]	SILT SOCK DITCH CHECK, SEE DETAIL SHEET TD-4
[Symbol]	SILT FENCE, SEE DETAIL SHEET TD-5

1. ALL RECOMMENDED GRADES ARE TO FINISHED YARD GRADE.
2. FINISHED GRADE ELEVATION (FVE) = (FVE) - (FINISH) + (FINISH).
3. TOP OF FOUNDATION WALL ELEVATION (FVE) = (FVE) + (FINISH).
4. CHANGES ARE SUBJECT TO CHANGE ORDERING ON LOCATION.
5. IF HOME AND SOIL CONDITIONS VARY FROM FIELD REPORTS, THE CONTRACTOR SHALL VERIFY THE LOCATION AND NOTIFY THE CITY OF LAKE GENEVA. EACH TIME A DRAWING IS REVISIONED DURING CONSTRUCTION.
6. ALL PROPOSED MOISTURE PRACTICES SHOULD BE LEFT LOWER THAN FINISHED GRADE AT THE CONTRACTOR'S DISCRETION TO ALLOW FOR BULKING MATERIAL. SPECIAL FINISHES, DISTRIBUTION.
7. ALL PROPOSED STONE CHECK DAMS TO BE PROTECTED PER DETAIL.

INSPECT ALL EROSION CONTROL MEASURES PRIOR TO COMMENCING GRADING. EROSION CONTROL MEASURES SHOULD BE MONITORED DURING CONSTRUCTION. MEASURES MUST BE INSPECTED WEEKLY AND WITHIN 24 HOURS OF EVERY PRECIPITATION EVENT OF 0.50 INCH OR GREATER. THE CONTRACTOR SHALL CONDUCT DAILY INSPECTIONS AND DOCUMENT CONDITIONS AND REPAIRS MADE ALONG WITH DATE, TIME OF INSPECTION AND WEATHER CONDITIONS BY A DAILY LOG BOOK. THE DAILY LOG BOOK, WEEKLY / 60 DAY PRECIPITATION REPORTS, APPROVED PLANS AND NOTES SHOULD BE KEPT IN AN ACCESSIBLE LOCATION, LIKE A BARRICADE WITHIN THE STAGING AREA.

AT ABSOLUTELY NO TIME MAY CONSTRUCTION EQUIPMENT, TOOLS, OR MATERIALS BE PLACED WITHIN WETLANDS, WETLANDS OR FLOOD PLAIN UNLESS IDENTIFIED IN THE PLANS.

DIGGERS HOTLINE
 Toll Free (800) 242-8511
 Milwaukee Area (414) 258-1181
 Hearing Impaired TDD (800) 642-2289
 www.DiggersHotline.com

SEE SHEET GP-2

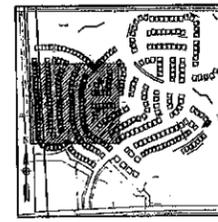
SEE SHEET GP-6

FILENAME: D:\Projects\Lake Geneva\GP-3\GP-3.dwg



SEE SHEET GP-4

SEE SHEET GP-5



KAPUR & ASSOCIATES, INC.
CONSULTING ENGINEERS
1711 N. PORT WASHINGTON ROAD
MILWAUKEE, WISCONSIN 53227
Phone: 414.831.8888 Fax: 414.831.4187
www.kapurengineers.com

PROJECT: SYMPHONY BAY
LOCATION: CITY OF LAKE GENEVA, WI

CLIENT:
RELEASE: PRELIMINARY NOT FOR CONSTRUCTION

REVISIONS:

NO.	DATE	DESCRIPTION
01	08/20/08	ISSUE FOR PERMIT
02	08/20/08	ISSUE FOR PERMIT
03	08/20/08	ISSUE FOR PERMIT
04	08/20/08	ISSUE FOR PERMIT
05	08/20/08	ISSUE FOR PERMIT
06	08/20/08	ISSUE FOR PERMIT
07	08/20/08	ISSUE FOR PERMIT
08	08/20/08	ISSUE FOR PERMIT
09	08/20/08	ISSUE FOR PERMIT
10	08/20/08	ISSUE FOR PERMIT



SCALE: 1" = 50'

SEAL:

DATE: 08/20/08

SHEET: GRADING AND EROSION CONTROL PLAN

PROJECT NUMBER: 15001
DATE: 1/22/08

SHEET NUMBER: GP-4

GRADING LEGEND

---	EXISTING CONTOUR MINOR
---	EXISTING CONTOUR MAJOR
---	PROPOSED CONTOUR MINOR
---	PROPOSED CONTOUR MAJOR
---	LOT CERT. ELEVATION
---	PROPOSED FINISHED GRADE/ MIN. TOP OF FOUNDATION WALL
---	LOOKOUT / WALKOUT

EROSION CONTROL LEGEND

[Symbol]	CLASS I, TYPE B EROSION MATTING SEE DETAIL SHEET TD-4
[Symbol]	PYRAMAT OR EQUIVALENT CLASS III, TYPE D MATTING W/ 4" MIN. TOPSOIL, SEED AND CLASS I, TYPE B EROSION MATTING
[Symbol]	MEDIUM DUTY BRIPAP SEE DETAIL SHEET TD-5
[Symbol]	SILT SOCK DITCH CHECK, SEE DETAIL SHEET TD-4
[Symbol]	SILT FENCE, SEE DETAIL SHEET TD-5

1. ALL RECONSTRUCTED GRASSES ARE TO FINISHED YARD GRADE.
2. FINISHED FLOOR ELEVATION: FIG. 4 (S/LT-VARIES)
3. TOP OF FOUNDATION WALL ELEVATION: FIG. 4 (S/LT-VARIES)
4. GRADES ARE SUBJECT TO CHANGE DEPENDING ON LOCATION OF HOUSE AND SOIL CONDITIONS UPON FURNISHED APPROVAL.
5. ALL ACCESSIBLE OPENINGS SHALL BE CONSTRUCTED AT THE TIME OF CURB PLACEMENT.
6. ALL EXISTING SITE FIELD DRAINAGE TILES WHICH ARE ENCOUNTERED DURING CONSTRUCTION OPERATIONS SHALL BE REPAIRED, RESTORED AND/OR CONNECTED TO THE PROPOSED STORM SEWER SYSTEMS.
7. THE CITY OF LAKE GENEVA EARLY TIE-IN DRAIN TILE IS ENCOUNTERED DURING CONSTRUCTION.
8. ALL PROPOSED EXPOSED PAIS AREAS SHOULD BE LEFT LOWER THAN FINISHED GRADE AT THE CONTRACTOR'S DISCRETION TO ALLOW FOR BASEMENT MATERIAL SPILLS DURING HOME CONSTRUCTION.
9. ALL PROPOSED STORM DRAIN INLET TO BE PROVIDED PER DETAIL.

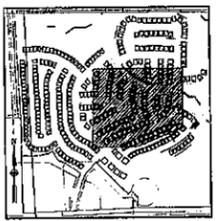
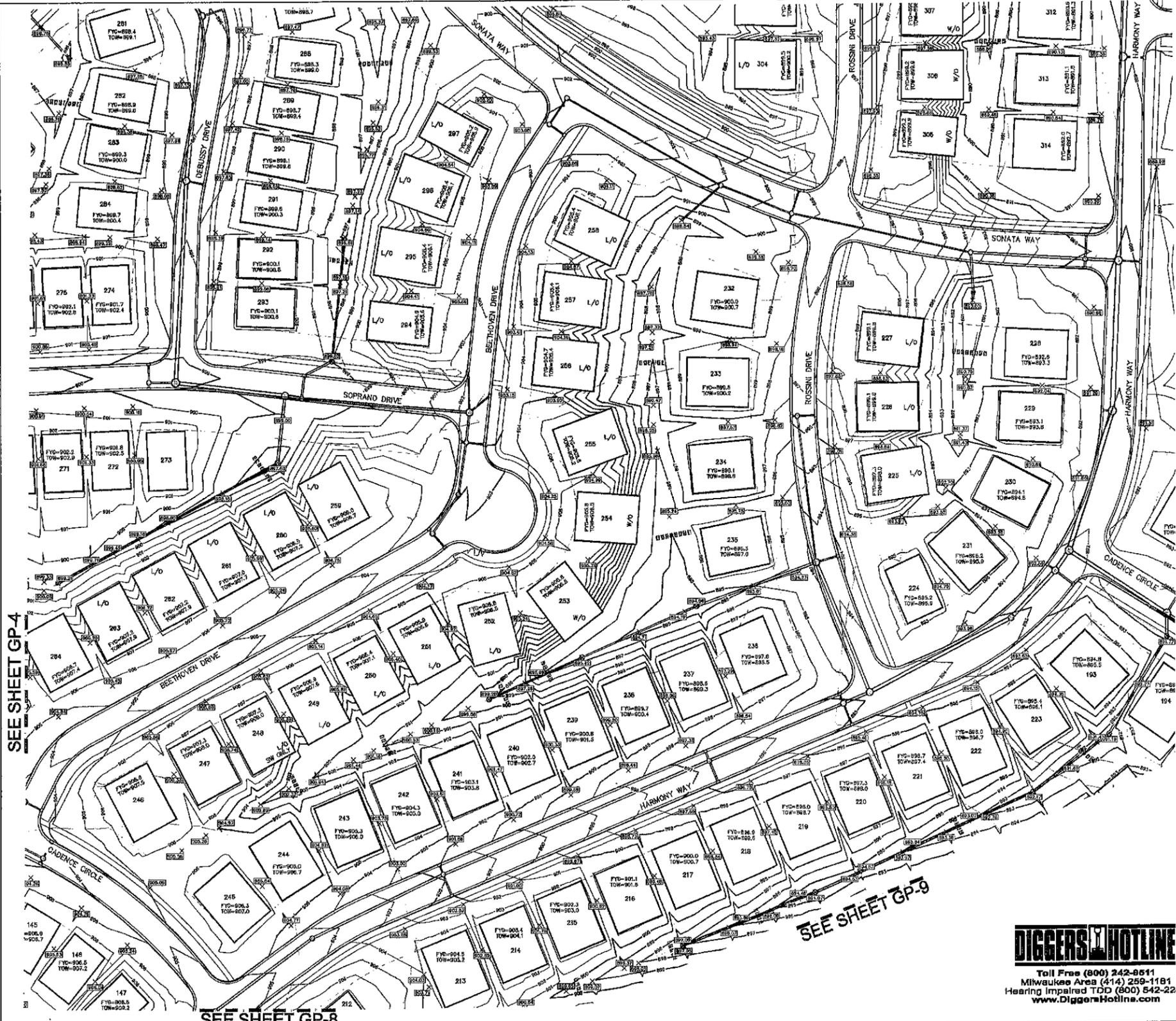
INSPECT ALL EROSION CONTROL MEASURES PRIOR TO COMMENCING GRADING OR STORM DRAIN OPERATIONS. EROSION CONTROL MEASURES MUST BE MAINTAINED THROUGHOUT CONSTRUCTION. CONTRACTOR SHALL CONDUCT EROSION CONTROL MEASUREMENTS AT THE TIME OF INSPECTION AND MAINTAIN CONTROLS IN A DAILY LOG. THE DAILY LOG SHALL BE KEPT IN AN ACCESSIBLE LOCATION, ONE A MINUTE, WITHIN THE STAGING AREA.

AT ABSOLUTELY NO TIME ANY CONSTRUCTION EQUIPMENT SHALL BE PLACED WITHIN WETLANDS, WATERWAYS OR FLOOD PLAINS UNLESS DENIED BY THE FPAAC.



Toll Free (800) 242-8811
Milwaukee Area (414) 250-1181
Hearing Impaired TDD (800) 642-2299
www.DiggersHotline.com

FILENAME: D:\Valeforth_COA\Lake Geneva_City\150001_Symphony Bay\Design\150001_GP_5\GP-4.dwg



KAPUR & ASSOCIATES, INC.
 CONSULTING ENGINEERS
 2711 N. POPE WASHINGTON ROAD
 MILWAUKEE, WISCONSIN 53211
 PHONE: 414.941.8500 FAX: 414.941.8117
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PROJECT: SYMPHONY BAY
 LOCATION: CITY OF LAKE GENEVA, WI

CLIENT:

RELEASE: PRELIMINARY NOT FOR CONSTRUCTION

REVISIONS:

#	DATE	DESCRIPTION



SCALE: 1" = 50'
 SEAL:

SHEET: GRADING AND EROSION CONTROL PLAN

PROJECT NUMBER: 18023
 DATE: 1-22-2010

SHEET NUMBER: GP-5

GRADING LEGEND

---	EXISTING CONTOUR MINOR
---	EXISTING CONTOUR MAJOR
---	PROPOSED CONTOUR MINOR
---	PROPOSED CONTOUR MAJOR
---	LOT CERT. ELEVATION
---	PROPOSED FINISHED GRADE / MIN. TOP OF FOUNDATION WALL
---	L/O W/O
---	LOOKOUT WALKOUT

EROSION CONTROL LEGEND

---	CLASS I, TYPE B EROSION MATTING SEE DETAIL SHEET TD-4
---	PYRAMAT OR EQUIVALENT CLASS III, TYPE D MATTING W/ 4" MIN. TOPSOIL, SEED AND CLASS I, TYPE B EROSION MATTING
---	MEDIUM DUTY BRUSHPAD SEE DETAIL SHEET TD-5
---	SILT SOCK DITCH CHECK, SEE DETAIL SHEET TD-4
---	SILT FENCE, SEE DETAIL SHEET TD-5

1. ALL UNDIMENSIONED GRADES ARE TO FINISHED TAKE GRADE.
2. FINISHED FLOOR ELEVATION = FFG ± FT (1.7'-VARIABLES)
3. TOP OF FOUNDATION WALL ELEVATION = FFG ± 2'
4. ROADS ARE SHOWN TO CHANGE DEPENDING ON LOCATION OF ROADS AND FOR CONSTRUCTION APPROVAL.
5. ALL ACCESSORY STRUCTURES SHALL BE CONSTRUCTED AT THE TIME OF CURB PLACEMENT.
6. ALL EXISTING SITE FIELD DRAINAGE TILES WHICH ARE ENCOUNTERED DURING CONSTRUCTION OPERATIONS SHALL BE REPAIRED, RESTORED AND/OR CONNTECTED TO THE PROPOSED STORM SEWER SYSTEM. THE CONSTRUCTION SHALL WORK UNDER THE SUPERVISION OF THE CITY OF LAKE GENEVA SUCH THAT A DRAIN TILE IS NOT ENCOUNTERED DURING CONSTRUCTION.
7. ALL PROPOSED HOUSE PAD AREAS SHOULD BE LEFT LOWER THAN FINISHED GRADE AT THE CONSTRUCTION BORDER TO ALLOW FOR BARRIAGE MATERIAL, SHOULD BARRIAGE CONSTRUCTION AND TRUCK TRAIL SHALL BE LEFT IN AN ACCESSIBLE LOCATION, LIKE A MAREAU, WITHIN THE STAGING AREA.
8. ALL PROPOSED STORM CATCH BASINS TO BE PROTECTED PER DETAIL.



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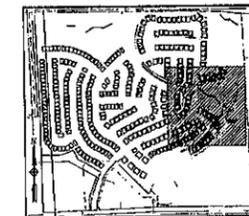
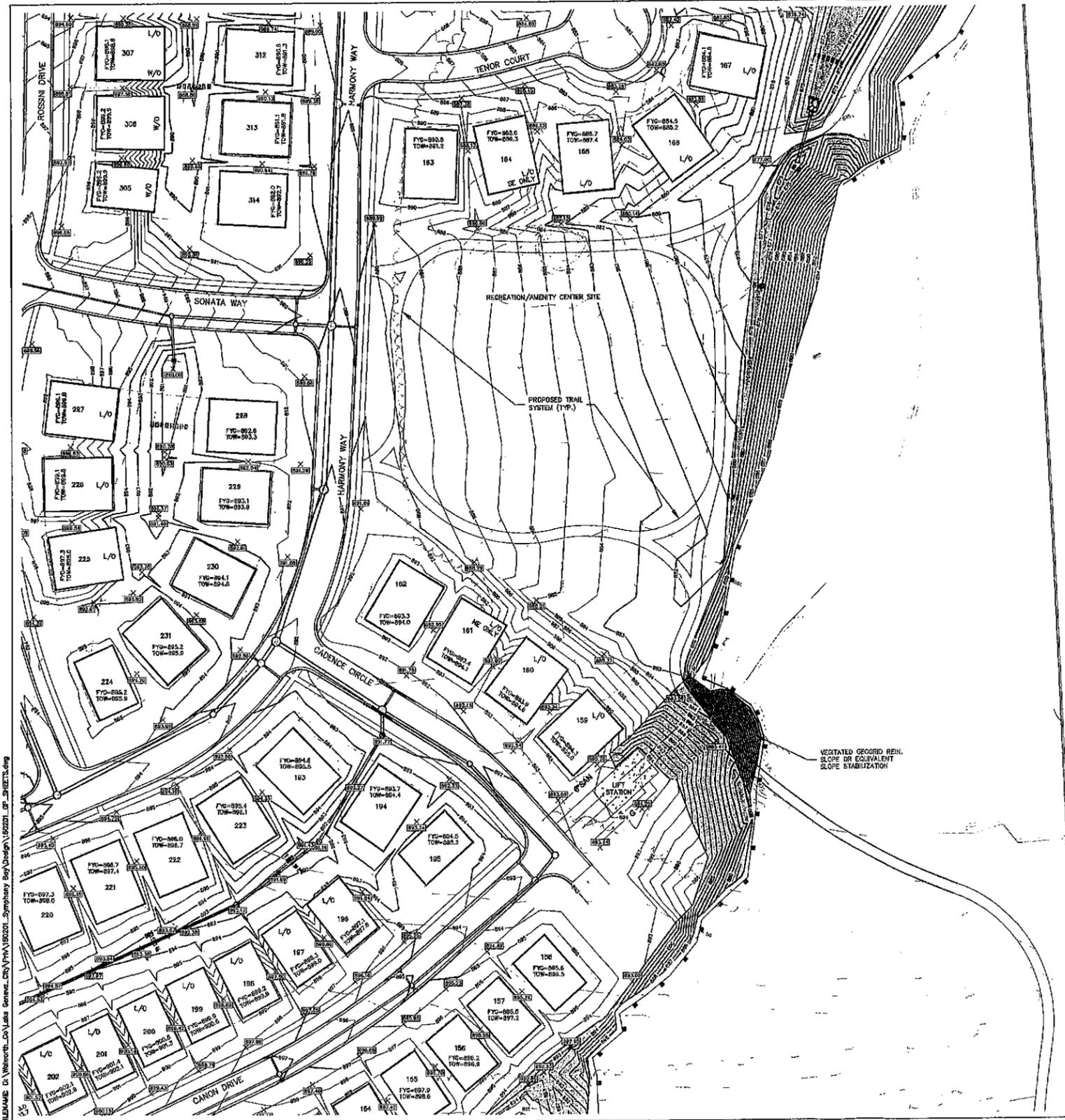
AT ABSOLUTELY NO TIME MAY CONSTRUCTION EQUIPMENT, DEBRIS, ETC. BE PLACED NEAR WEATHER BARRIAGES OR DRAIN PLANS UNLESS SHOWN IN THE PLANS.

SEE SHEET GP-4

SEE SHEET GP-8

SEE SHEET GP-9

FILENAME: D:\winners\do\lake_geneva\cra\pva\1802301_symp_hay\design\1802301_gp_sheets.dwg



GRADING LEGEND

999	EXISTING CONTOUR MINOR
--- 999 ---	EXISTING CONTOUR MAJOR
--- 999 ---	PROPOSED CONTOUR MINOR
--- 999 ---	PROPOSED CONTOUR MAJOR
(887.47)	LOT CERT. ELEVATION
F70-888.3 TOW-889.0	PROPOSED FINISHED GRADE/ MIN. TOP OF FOUNDATION WALL
L/O W/O	LOOKOUT WALKOUT

EROSION CONTROL LEGEND

[Symbol]	CLASS I, TYPE B EROSION MATTING SEE DETAIL SHEET TD-4
[Symbol]	PYRAMID OR EQUIVALENT CLASS III, TYPE D MATTING W/ 4" MIN. TOPSOIL, SEED AND CLASS I, TYPE B EROSION MATTING
[Symbol]	MEDIUM DUTY REGRAP SEE DETAIL SHEET TD-5
100BDD	SILT SOCK DITCH CHECK. SEE DETAIL SHEET TD-4
[Symbol]	SILT FENCE. SEE DETAIL SHEET TD-5

1. VERIFY ALL EROSION CONTROL MEASURES FROM 18 CONSECUTIVE GRADING
SWATHS OR OTHER LAND DISTURBED ACTIVITIES. EROSION CONTROL MEASURES
MAY BE INSPECTED VISUALLY AND WITHIN 24 HOURS OF EROSION PREVENTION
EARTH OF ONE INCH OR GREATER. IN ADDITION THE CONTRACTOR SHALL CONDUCT
DAILY INSPECTIONS AND MEASUREMENTS OF EROSION CONTROL MEASURES
DURING PERIODS OF HEAVY RAIN AND WEATHER CONDITIONS IN A DAILY LOG BOOK. THE
DAILY LOG BOOK, VISUALLY, AND WITHIN 24 HOURS OF EROSION PREVENTION, APPROVED PLANS
AND PHOTO EVIDENCE SHALL BE KEPT IN AN ACCESSIBLE LOCATION, LIKE A MAILBOX,
NEAR THE WORKING AREA.

2. VERIFY ALL EROSION CONTROL MEASURES FROM 18 CONSECUTIVE GRADING
SWATHS OR OTHER LAND DISTURBED ACTIVITIES. EROSION CONTROL MEASURES
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NEAR THE WORKING AREA.

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PROJECT:
SYMPHONY BAY

LOCATION:
CITY OF LAKE GENEVA, WI

CLIENT:

RELEASE:
PRELIMINARY NOT FOR CONSTRUCTION

REVISIONS:

#	DATE	DESCRIPTION



SCALE: 1" = 50'

SEAL:

SHEET:
GRADING AND EROSION CONTROL PLAN

PROJECT MANAGER:
PROJECT NUMBER: 15091
DATE: 1/22/2010

SHEET NUMBER:
GP-6

FILENAME: C:\Work\05_Vis\05_Lake Geneva_City\15091\15091_Symphony Bay\Design\15091_01_SHEETS.dwg



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PROJECT:
SYMPHONY BAY

LOCATION:
CITY OF LAKE GENEVA, WI

CURBIT:

RELEASE:
PRELIMINARY NOT FOR CONSTRUCTION

REVISIONS:

#	DATE	DESCRIPTION
1	08/21/07	ISSUE FOR PERMIT
2	08/21/07	ISSUE FOR PERMIT
3	08/21/07	ISSUE FOR PERMIT
4	08/21/07	ISSUE FOR PERMIT
5	08/21/07	ISSUE FOR PERMIT
6	08/21/07	ISSUE FOR PERMIT
7	08/21/07	ISSUE FOR PERMIT
8	08/21/07	ISSUE FOR PERMIT
9	08/21/07	ISSUE FOR PERMIT
10	08/21/07	ISSUE FOR PERMIT



SCALE: 1" = 40'

SEAL:

SHEET:
GRADING AND EROSION CONTROL PLAN

PROJECT NUMBER: 15001
 DATE: 1/22/08

SHEET NUMBER:
GP-7

GRADING LEGEND

---	EXISTING CONTOUR MINOR
---	EXISTING CONTOUR MAJOR
---	PROPOSED CONTOUR MINOR
---	PROPOSED CONTOUR MAJOR
---	LOT CERT. ELEVATION
---	PROPOSED FINISHED GRADE/ 1/4" MIN. TOP OF FOUNDATION WALL
---	LOOKOUT W/O

EROSION CONTROL LEGEND

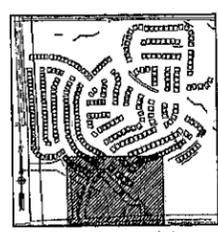
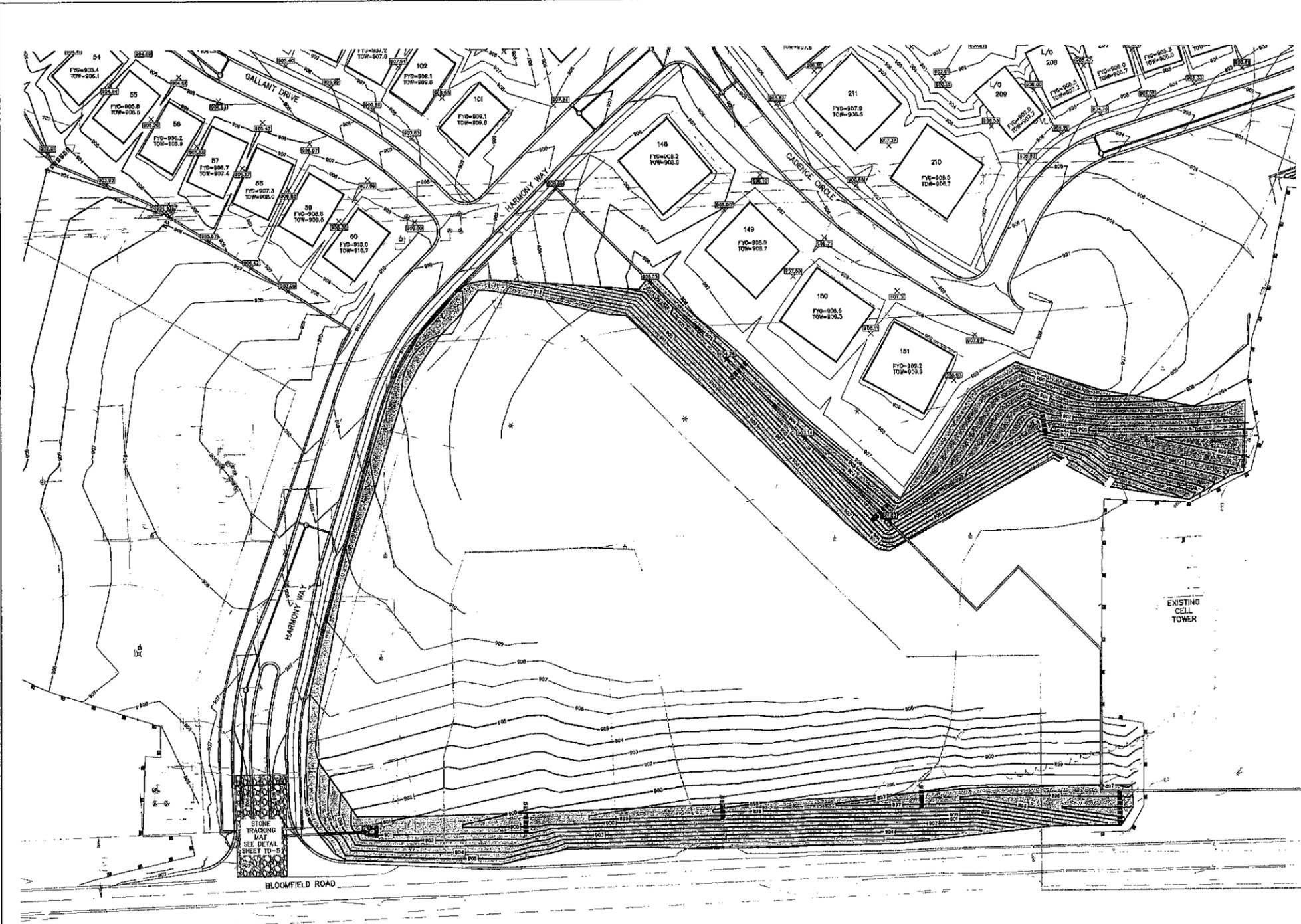
[Symbol]	CLASS I, TYPE B EROSION MATTING SEE DETAIL SHEET TD-4
[Symbol]	PYRAMAT OR EQUIVALENT CLASS II, TYPE D MATTING W/ 4" MIN. TOPSOIL, SEED AND CLASS I, TYPE B EROSION MATTING
[Symbol]	MEDIUM DUTY RIPRAP SEE DETAIL SHEET TD-5
[Symbol]	SILT SOCK DITCH CHECK, SEE DETAIL SHEET TD-4
[Symbol]	SILT FENCE, SEE DETAIL SHEET TD-5

1. ALL EROSION CONTROL MEASURES SHALL BE INSTALLED PRIOR TO COMMENCING CONSTRUCTION AND MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD.
2. THE CONTRACTOR SHALL MAINTAIN ALL EROSION CONTROL MEASURES IN GOOD WORKING ORDER AT ALL TIMES.
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PROJECT: SYMPHONY BAY
 LOCATION: CITY OF LAKE GENEVA, WI

CLIENT:

RELEASE: PRELIMINARY NOT FOR CONSTRUCTION

REVISIONS:

#	DATE	DESCRIPTION
001	08/21/08	ISSUE
002	08/21/08	ISSUE
003	08/21/08	ISSUE
004	08/21/08	ISSUE
005	08/21/08	ISSUE
006	08/21/08	ISSUE
007	08/21/08	ISSUE
008	08/21/08	ISSUE
009	08/21/08	ISSUE
010	08/21/08	ISSUE



SCALE: 1"=50'

SEAL:

SHEET: GRADING AND EROSION CONTROL PLAN

PROJECT NUMBER: 15001
 PROJECT NUMBER: 15001
 DATE: 1-22-2008

SHEET NUMBER: GP-8

FILENAME: D:\Work\08_15\08\01\Symphony Bay\Design\150001.dwg SHEETS.dwg

1. ALL RECOMMENDED GRADES ARE TO FINISHED YARD GRADE.
2. FINISHED FLOOR ELEVATION: FIN + 1.5X1.75' MINIMUM.
3. TOP OF FOUNDATION WALL ELEVATION: FIN + 8" GRADES ARE SUBJECT TO CHANGE DEPENDING ON LOCATION OF HOUSE AND USE CONDITIONS UPON ENGINEER'S APPROVAL.
4. ANY ACCESSIBLE FLOORING SHALL BE CONSTRUCTED AT THE TIME OF CURB PLACEMENT.
5. ALL EXISTING SITE FIELD DRAINAGE TILES WHICH ARE ENCOUNTERED DURING CONSTRUCTION OPERATIONS SHALL BE REPAIRED, REROUTED AND/OR COVERED TO THE PROPOSED STONE BARRIER.
6. THE CONTRACTOR SHALL MAINTAIN SUFFICIENT TIE LOCATIONS AND NOTES THE CITY OF LAKE GENEVA EACH TIME A BARRIER TILE IS ENCOUNTERED DURING CONSTRUCTION.
7. ALL PROPOSED BRIDGE PAVEMENT SHALL BE LEFT LOWER THAN FINISHED GRADE AT THE CONSTRUCTION LOCATION IN ORDER TO ALLOW FOR DRAINAGE MATERIAL TO BE PLACED UNDER CONSTRUCTION.
8. ALL PROPOSED STONE CATCH BASINS TO BE PROVIDED PER DETAIL.

EROSION CONTROL LEGEND

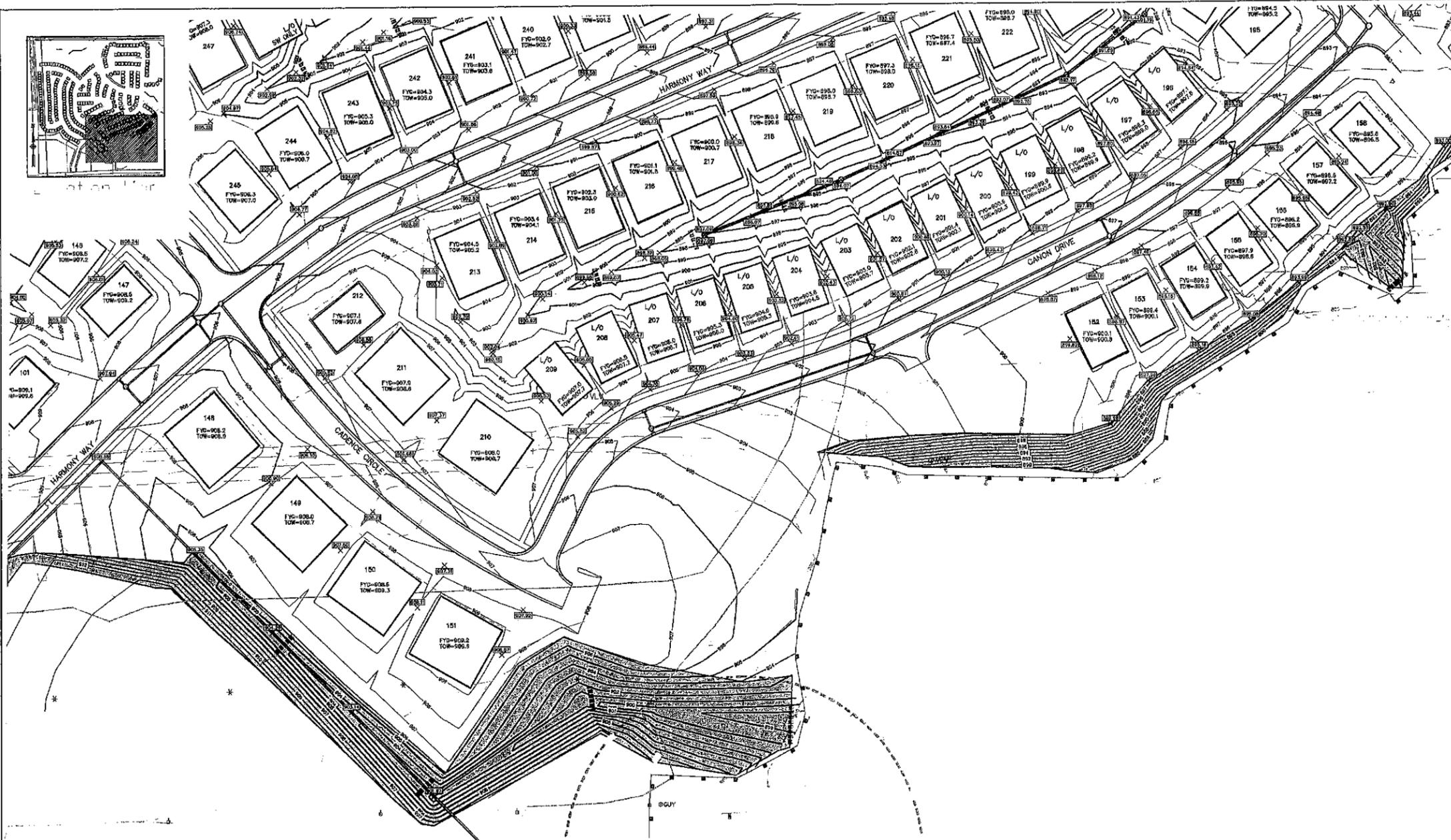
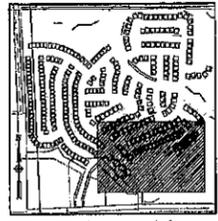
	CLASS I, TYPE B EROSION MATTING SEE DETAIL SHEET TD-4
	PYRAMAT OR EQUIVALENT CLASS III, TYPE D MATTING W/ 4" MIN. TOPSOIL, SEED AND CLASS I, TYPE B EROSION MATTING
	MEDIUM DUTY RIPRAP SEE DETAIL SHEET TD-5
	SILT SOCK DITCH CHECK, SEE DETAIL SHEET TD-4
	SILT FENCE, SEE DETAIL SHEET TD-5

GRADING LEGEND

	EXISTING CONTOUR MINOR
	EXISTING CONTOUR MAJOR
	PROPOSED CONTOUR MINOR
	PROPOSED CONTOUR MAJOR
	LOT CERT. ELEVATION
	PROPOSED FINISHED GRADE/ 1/2" MIN. TOP OF FOUNDATION WALL
	LOOKOUT WALKOUT

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INSPECT ALL EROSION CONTROL MEASURES PRIOR TO COMMENCING GRADING. CONTINUE OR STOP WORK DEPENDING ON WEATHER CONDITIONS. WEATHERS SHALL BE MONITORED. IF WEATHERS ARE NOT SATISFACTORY, THE CONTRACTOR SHALL CONDUCT A RAIN LOG. THE RAIN LOG SHALL BE KEPT IN AN ACCESSIBLE LOCATION, LIKE A MAILBOX, DURING THE CONSTRUCTION PERIOD. THE RAIN LOG SHALL BE KEPT IN AN ACCESSIBLE LOCATION, LIKE A MAILBOX, DURING THE CONSTRUCTION PERIOD.



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SYMPHONY BAY

LOCATION:
CITY OF LAKE GENEVA, WI

CLIENT:

RELEASE:
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REVISIONS:

#	DATE	DESCRIPTION



SCALE: 1" = 50'

SEAL:

SHEET:
GRADING AND EROSION CONTROL PLAN

PROJECT MANAGER: _____
PROJECT NUMBER: 190291
DATE: 1-22-2016

SHEET NUMBER:
GP-9

FILENAME: D:\work\16\160291\160291_Symphony Bay\Design\160291_GP_9_SHEETS.dwg

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AT ABSOLUTELY NO TIME MAY CONSTRUCTION EQUIPMENT, DEBRIS, FILL, ETC. BE PLACED WITHIN WETLANDS, WATERWAYS OR FLOOD PLAINS UNLESS IDENTIFIED IN THE PLANS.

1. ALL RECOMMENDED GRADES ARE TO FINISHED YARD GRADE.
2. FINISHED FLOOR ELEVATION = FIC ± 1" (1/2" - 1/4")
3. TOP OF FOUNDATION WALL ELEVATION = FIC ± 1"
4. CHANGES ARE SUBJECT TO CHANGE DEPENDING ON LOCATION OF ROADS AND SOIL CONDITIONS UPON ENGINEER'S APPROVAL.
5. ALL ACCESSIBLE OPENINGS SHALL BE CONSTRUCTED AT THE TIME OF CURB PLACEMENT.
6. ALL EXISTING SITE FILL DRAINAGE TILES WHICH ARE DISCONTINUED DURING CONSTRUCTION OPERATIONS SHALL BE REPAIRED, SUPPORTED AND/OR CONNECTED TO THE PROPOSED STORM SEWER SYSTEM. THE CONTRACTOR SHALL MAINTAIN SUFFICIENT THE LOCATION AND NOTIFY THE CITY OF LAKE GENEVA EACH TIME A TRENCH IS TO BE EXCAVATED DURING CONSTRUCTION.
7. ALL PROPOSED HOUSE PALE AREAS SHOULD BE LEFT UNOCCUPIED UNTIL FINISHED GRADE. ALL THE EXCAVATIONS SHOULD BE ALLOWED FOR EROSION CONTROL MEASURES DURING CONSTRUCTION.
8. ALL PROPOSED STORM DRAIN BASINS TO BE PROTECTED PER DETAIL.

EROSION CONTROL LEGEND

	CLASS I, TYPE B EROSION MATTING SEE DETAIL SHEET TD-4
	PYRAMID OR EQUIVALENT CLASS II, TYPE D MATTING W/ 4" MIN. TOPSOIL, SEED AND CLASS I, TYPE B EROSION MATTING
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	SILT SOCKET DITCH CHECK, SEE DETAIL SHEET TD-4
	SILT FENCE, SEE DETAIL SHEET TD-5

GRADING LEGEND

	999 EXISTING CONTOUR MINOR
	999 EXISTING CONTOUR MAJOR
	999 PROPOSED CONTOUR MINOR
	999 PROPOSED CONTOUR MAJOR
	897.47 LOT CERT. ELEVATION
	FYG=898.3 TOW=899.0 PROPOSED FINISHED GRADE/ MIN. TOP OF FOUNDATION WALL
	L/O W/O LOOKOUT WALKOUT

TRAFFIC STUDY

TRAFFIC IMPACT ANALYSIS FOR:

SYMPHONY BAY DEVELOPMENT
Townline Road/Bloomfield Road

CITY OF LAKE GENEVA, WALWORTH COUNTY, WISCONSIN

DATE SUBMITTED: November 16, 2015

PREPARED BY:

Traffic Analysis & Design, Inc.

N36 W7505 Buchanan Street

Lake Geneva, WI 53012

Phone: (800) 605-3091

Contact Persons: Don Lee, P.E.

(WisDOT TIA Certification # SE05-804-046)

John Bieberitz, P.E., PTOE

(WisDOT TIA Certification # SE05-804-044)

"I certify that this Traffic Impact Analysis has been prepared by me or under my immediate supervision and that I have experience and training in the field of traffic and transportation engineering."

Donald J. Lee, P.E.

Wisconsin Registration #35214-006

Traffic Analysis & Design, Inc.

**Symphony Bay Development
Traffic Impact Analysis
Table of Contents**

LIST OF EXHIBITS.....	ii
LIST OF APPENDICES.....	iii
CHAPTER I – INTRODUCTION & EXECUTIVE SUMMARY.....	1
Part A – Purpose of Report and Study Objectives.....	1
Part B – Executive Summary.....	1
CHAPTER II – PROPOSED DEVELOPMENT.....	3
Part A – Proposed Development.....	3
Part B – Study Area.....	3
Part C – Site Accessibility.....	3
CHAPTER III – EXISTING CONDITIONS.....	4
Part A – Physical Characteristics.....	4
Part B – Traffic Volumes.....	4
Part C – Sources of Data.....	4
CHAPTER IV – DEVELOPMENT TRAFFIC.....	5
Part A – Proposed Site Traffic Forecasting.....	5
Part B – Build (With development) Traffic.....	5
Part C – Development Level of Service.....	5
Part D – Queueing Analysis.....	6
CHAPTER V – RECOMMENDATIONS AND CONCLUSION.....	7
Part A – Recommendations.....	7
Part B – Conclusion.....	7

LIST OF EXHIBITS

- Exhibit 1-1Project Location Map
- Exhibit 1-2Conceptual Site Plan
- Exhibit 1-3Year 2016 Recommended Improvements

- Exhibit 2-1Project Location Map
- Exhibit 2-2Conceptual Site Plan

- Exhibit 3-1Existing Transportation Detail
- Exhibit 3-2Existing Traffic Volumes

- Exhibit 4-1Symphony Bay Trip Generation and Distribution
- Exhibit 4-2Symphony Bay New Trips
- Exhibit 4-3Year 2016 Build Traffic Volumes
- Exhibit 4-4Year 2016 Build Traffic Operations – With Improvements
- Exhibit 4-5Year 2016 Build Traffic – Expected Maximum Queues

LIST OF APPENDICES

Appendix A...Traffic

Existing Turning Movement Counts

Appendix B...Peak Hour Analysis Outputs

Year 2016 Build (with development) Traffic

Year 2016 Build (with development) Traffic with Improvements

CHAPTER I – INTRODUCTION & EXECUTIVE SUMMARY

PART A – PURPOSE OF REPORT AND STUDY OBJECTIVES

The City of Lake Geneva has requested a Traffic Impact Analysis (TIA) to analyze the impacts of the planned senior residential Symphony Bay development located between Bloomfield Road and Townline Road, approximately ¼ mile east of State Trunk Highway (STH) 120 in the City of Lake Geneva, Walworth County, Wisconsin. Traffic Analysis & Design, Inc. has been retained to identify the existing traffic operating conditions, the Year 2016 Build (with development) traffic operating conditions and improvements necessary to provide safe and efficient traffic operations under full build conditions.

This report documents the procedures, findings and conclusions of the traffic impact analysis. The analysis investigates improvements based on existing intersection geometrics, background traffic volumes and additional traffic expected to be generated by the proposed development within the limits of the study area.

PART B – EXECUTIVE SUMMARY

The executive summary includes a description of the study area, description of the potential expansion plans and conclusions based on the findings of the supplemental TIA.

B1. Location of Study Site with Respect to Area Roadway Network

The proposed senior residential development is located between Bloomfield Road and Townline Road, approximately ¼ mile east of STH 120 in the City of Lake Geneva as shown in Exhibit 1-1. Exhibit 1-2 shows the proposed conceptual site plan for the Symphony Bay residential development. Access to the proposed development site is proposed as two new residential streets. The first, Sonata Way, is proposed as a full access street onto Townline Road and is proposed to be located approximately 1/3-mile east of STH 120. A second access, Harmony Drive, is proposed as a full access street onto Bloomfield Road and is proposed to be located approximately 1/4-mile east of STH 120. Finally, a potential future access to the west to provide cross access with future commercial uses along STH 120 is also considered on the conceptual site plan. Based on discussions with the City of Lake Geneva and as shown in Exhibit 1-1, the study area for the proposed residential development includes the following intersections:

- Townline Road & proposed Sonata Way (one-way stop control);
- Bloomfield Road & proposed Harmony Drive (one-way stop control).

B2. Development Description and Timing

The Symphony Bay residential development is proposed to include 374 senior single-family residential parcels and 58 side by side duplex units (on 29 parcels). In addition, the development plans to include a senior housing facility and a church on two of the southern parcels. The senior housing facility is expected to include a total of 100 units comprised of independent living, assisted living and memory care units. The church is expected to accommodate approximately 250 people. For purposes of this traffic study, full build out of the entire residential development is expected to be completed in 2016. The conceptual site plan for the proposed Symphony Bay development is shown in Exhibit 1-2.

B3. Year 2016 Build (With development) Traffic – Recommended Improvements

The study area intersections were analyzed based on the procedures set forth in the *2010 Highway Capacity Manual (HCM)*. Intersection operation is defined by “level of service”. Level of Service (LOS) is a quantitative measure that refers to the overall quality of flow at an intersection ranging from very good, represented by LOS ‘A’, to very poor, represented by LOS

'F'. For the purpose of this study, LOS D or better was used to define desirable peak hour operating conditions.

Year 2016 Build (with development) traffic volumes include the full build out of the Symphony Bay development. Based on the analysis completed as part of this study, the following improvements, as shown on Exhibit 1-3, are recommended to accommodate the Year 2016 Build (with development) traffic volumes.

Bloomfield Road & proposed Harmony Drive

- Construct residential street with one entrance lane and one exit lane;
- Provide stop sign control on north approach only.

Townline Road & proposed Sonata Way

- Construct residential street with one entrance lane and one exit lane;
- Provide stop sign control on south approach only.

It is noted that the construction of an eastbound bypass lane on Bloomfield Road and a westbound bypass lane on Townline Road were investigated at the two development access roadways and it was determined that a bypass lane was not needed from an operations (LOS) perspective since all movements are expected to operate at LOS B or better under the year 2016 Build conditions. In addition, based on by-pass guidelines utilized in other neighboring counties, since the daily volumes on the side road are not expected to exceed 2,500-vehicles per day, a bypass lane is not expected to be necessary at the two development access roadways.

With the recommended improvements, all movements at the study area intersections are expected to operate at acceptable levels under the Year 2016 Build (with development) traffic conditions.

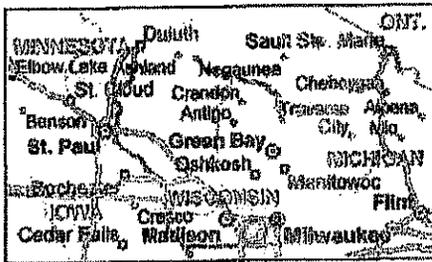
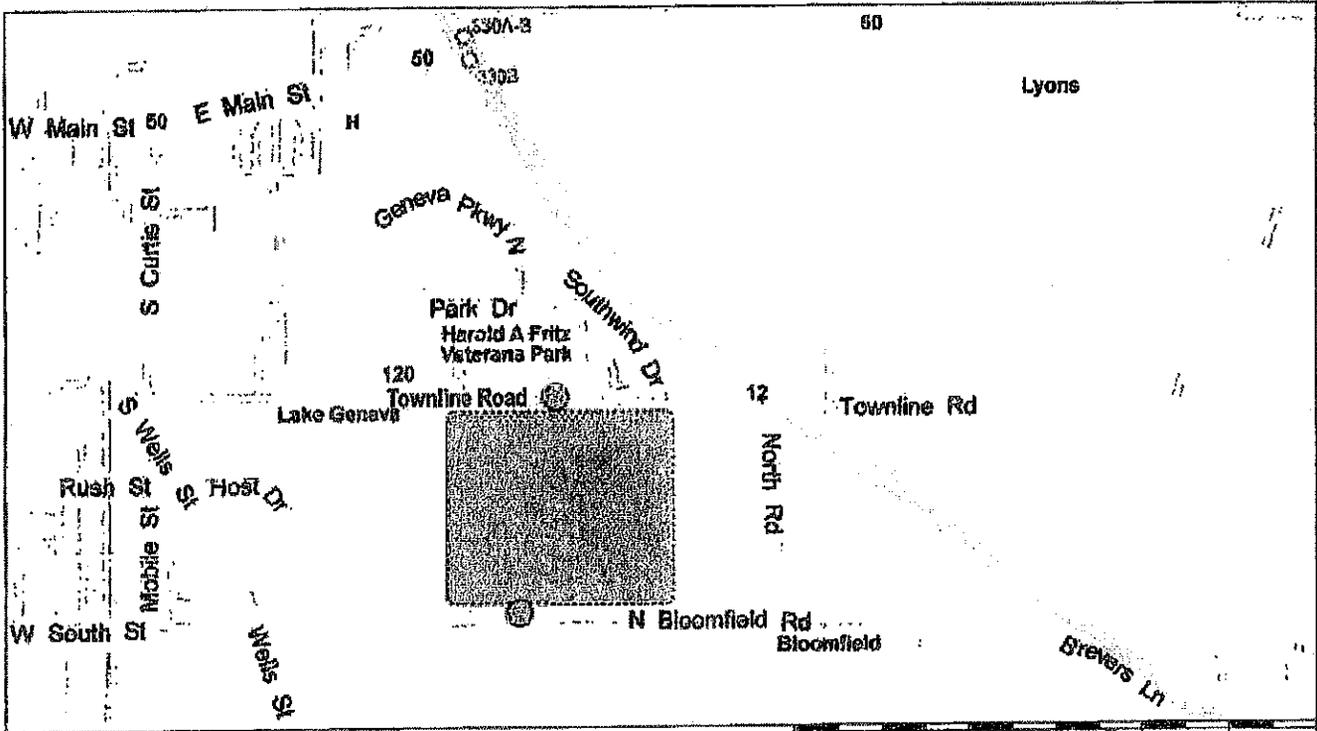
B4. Conclusion

All movements at the study area intersections are expected to operate safely and efficiently through the construction year 2016 with the recommended improvements under full build conditions.



NOT TO SCALE

Lake Geneva, Wisconsin



LEGEND

-  Study Area Intersection
-  Proposed Development

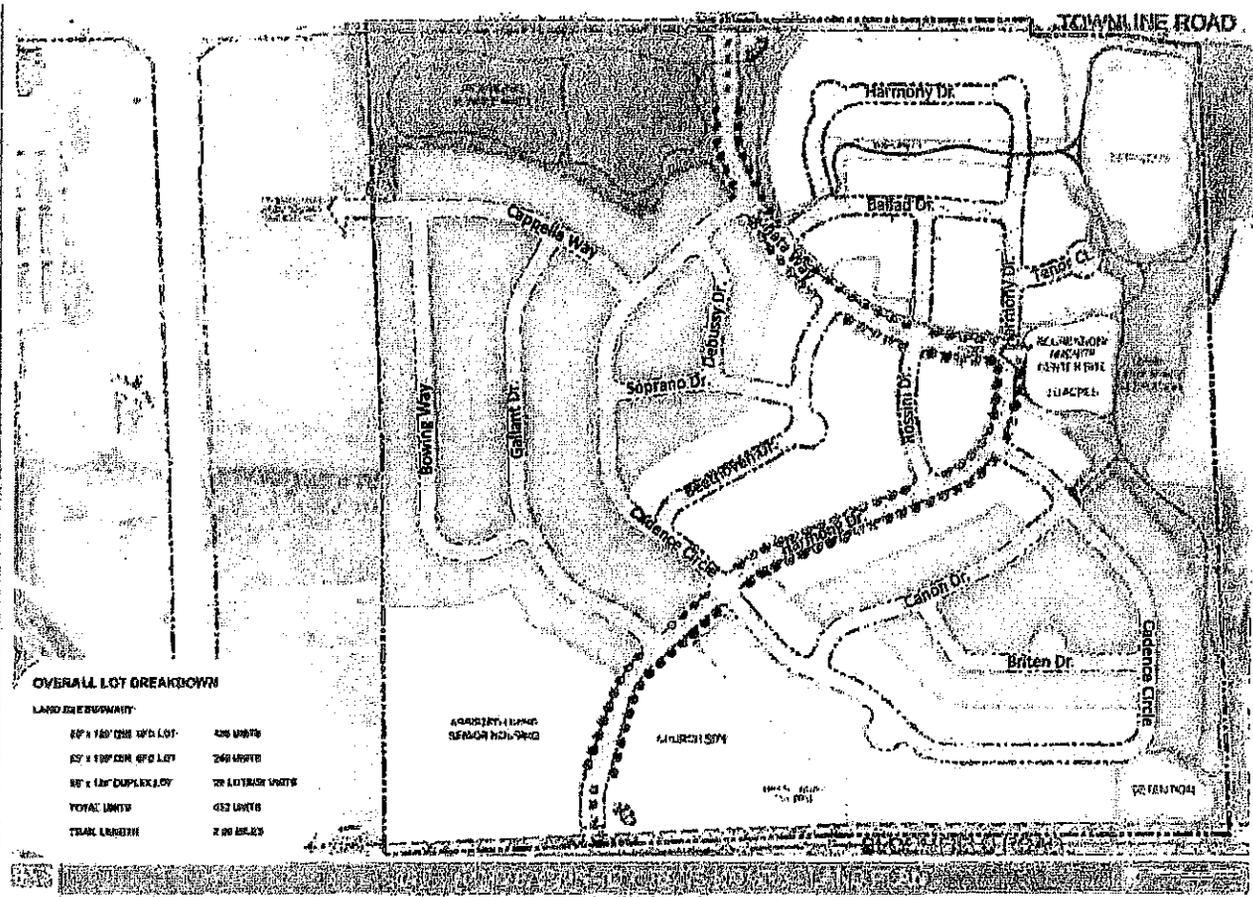
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EXHIBIT 1-1 PROJECT LOCATION MAP

SYMPHONY BAY RESIDENTIAL DEVELOPMENT LAKE GENEVA, WISCONSIN

Symphony Bay—Lake Geneva, WI



OVERALL LOT BREAKDOWN

LAND USE SUMMARY	
60' x 150' COR. 1/2 D. LOT	426 UNITS
60' x 150' COR. 3/4 D. LOT	246 UNITS
80' x 120' COMPLEX LOT	20 LUXURY UNITS
TOTAL UNITS	692 UNITS
TOTAL LENGTH	2.80 MILES



**EXHIBIT 1-2
CONCEPTUAL SITE PLAN**

**SYMPHONY BAY RESIDENTIAL DEVELOPMENT
LAKE GENEVA, WISCONSIN**

LEGEND

-  Stop Sign
-  Existing Lane Configuration
-  Proposed Lane Configuration
-  Proposed Roadway Connection
-  Divided Roadway



120
H

Townline Road

Bloomfield Road

Edwards Boulevard

Sonata Way

Harmony Drive



EXHIBIT 1-3
FULL BUILD RECOMMENDED IMPROVEMENTS
SYMPHONY BAY RESIDENTIAL DEVELOPMENT
LAKE GENEVA, WISCONSIN

CHAPTER II – PROPOSED DEVELOPMENT

PART A – PROPOSED DEVELOPMENT

The proposed senior residential development is located between Bloomfield Road and Townline Road, approximately ¼ mile east of STH 120 in the City of Lake Geneva as shown in Exhibit 2-1. The development site currently consists of farm fields, open fields and wooded areas as well as a vacant farmstead. The site is bordered by sports fields and residential neighborhoods to the north and farm fields to the east, west and south.

The planned development includes the construction of 374 senior single-family residential parcels and 58 side by side duplex units (on 29 parcels). In addition, the development plans to include a senior housing facility and a church on two of the southern parcels. The senior housing facility is expected to include a total of 100 units comprised of independent living, assisted living and memory care units. The church is expected to accommodate approximately 250 people. For purposes of this traffic study, full build out of the entire residential development is expected to be completed in 2016. Exhibit 2-2 shows the proposed conceptual site plan for the Symphony Bay residential development.

Full build out of the entire residential development is expected to be completed in 2016.

PART B – STUDY AREA

Based on discussions with the City of Lake Geneva and as shown in Exhibit 2-1, the study area for the proposed residential development includes the following intersections:

- Townline Road & proposed Sonata Way (one-way stop control);
- Bloomfield Road & proposed Harmony Drive (one-way stop control).

PART C – SITE ACCESSIBILITY

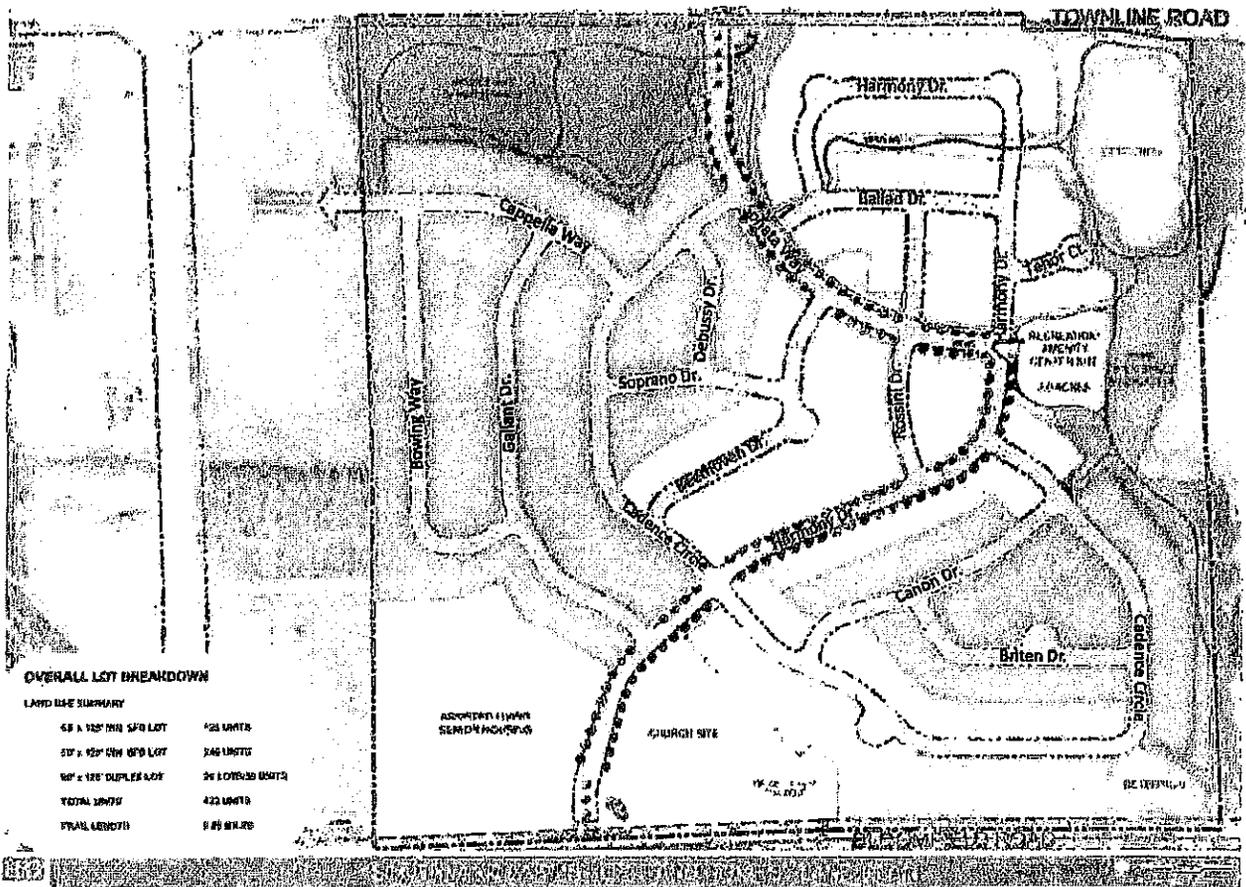
The study area roadways are discussed below:

Bloomfield Road is a two-lane undivided east/west rural roadway with a posted speed limit of 35 miles per-hour (mph) east of STH 120. According to the WisDOT, the Year 2009 annual average daily traffic (AADT) volumes on Bloomfield Road were 1,400-vpd east of STH 120. Sidewalks don't currently exist along Bloomfield Road within the limits of the proposed development; however, sidewalks do exist on the south side of Bloomfield Road, west of STH 120.

Townline Road is a two-lane undivided east/west rural roadway with a posted speed limit of 35-mph east of STH 120. Currently there is no AADT volumes collected on Townline Road within the limits of the proposed development. Sidewalks are present along the north side of Townline Road from west of STH 120 through South Wind Drive.

Access to the proposed development site is proposed as two new residential streets. The first, Sonata Way, is proposed as a full access street onto Townline Road and is proposed to be located approximately 1/3-mile east of STH 120. A second access, Harmony Drive, is proposed as a full access street onto Bloomfield Road and is proposed to be located approximately 1/4-mile east of STH 120. Finally, a potential future access to the west to provide cross access with future commercial uses along STH 120 is also considered on the conceptual site plan in Exhibit 2-2.

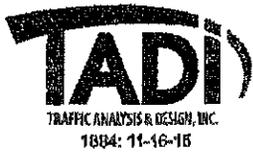
Symphony Bay—Lake Geneva, WI



OVERALL LOT BREAKDOWN

LAND USE SUMMARY

48 x 120' MID. SPD LOT	128 UNITS
50' x 120' GEN. SPD LOT	246 UNITS
60' x 120' DUPLEX LOT	24 1-OR-2-OR-3 UNITS
TOTAL UNITS	428 UNITS
TOTAL ACRES	8.25 ACRES



**EXHIBIT 2-2
CONCEPTUAL SITE PLAN**

**SYMPHONY BAY RESIDENTIAL DEVELOPMENT
LAKE GENEVA, WISCONSIN**

CHAPTER III – EXISTING CONDITIONS

PART A – PHYSICAL CHARACTERISTICS

Exhibit 3-1 shows the existing transportation detail for the study area intersections. More specifically, the exhibit illustrates intersection lane configurations, intersection traffic controls, posted speed limits, and approximate intersection spacing.

PART B – TRAFFIC VOLUMES

Existing Traffic Volumes

Weekday morning peak period (6:00 to 9:00 am) and weekday evening peak hour (3:00 to 6:00 pm) turning movement counts were conducted on a typical weekday in early November of 2015 at the Edwards Boulevard (STH 120) intersections with Bloomfield Road and Townline Road. Based on the turning movement counts, the weekday morning and weekday evening peak hours were identified as being 7:00 to 8:00 am and 3:30 to 4:30 pm; respectively. The existing traffic volumes at the two study area intersections are shown in Exhibit 3-2. The traffic counts used to determine peak hour factors and truck percentages have been included in the Appendix of this study.

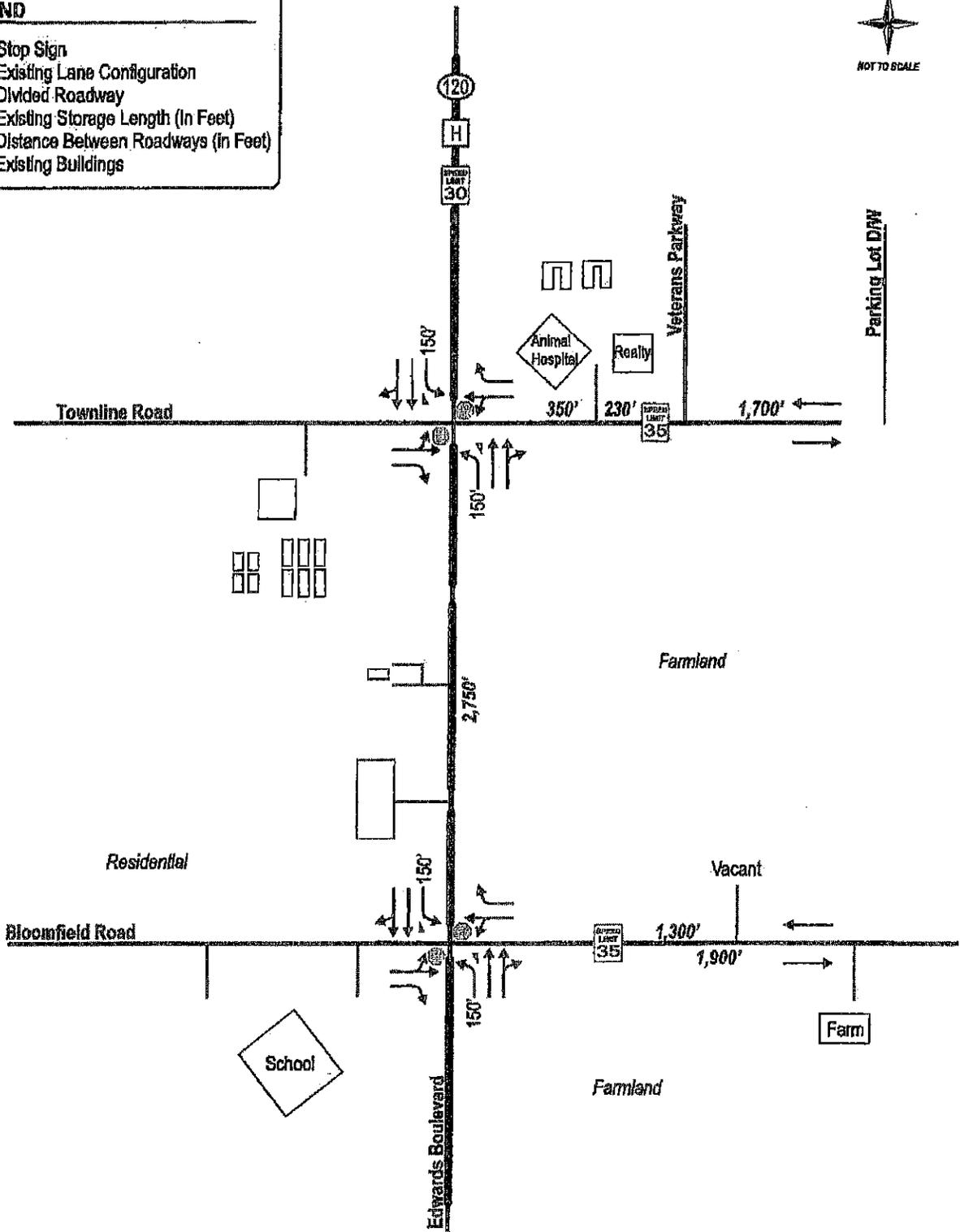
PART C – SOURCES OF DATA

The following sources of data were obtained for use in conducting this traffic study:

- Turning movement traffic counts – Traffic Analysis & Design, Inc.;
- Proposed Site Plan – FairWyn, via City of Lake Geneva;
- Existing transportation details – Traffic Analysis & Design, Inc. and Google™ Earth.

LEGEND

-  Stop Sign
-  Existing Lane Configuration
-  Divided Roadway
-  Existing Storage Length (In Feet)
-  Distance Between Roadways (In Feet)
-  Existing Buildings



**EXHIBIT 3-1
EXISTING TRANSPORTATION DETAIL**

**SYMPHONY BAY RESIDENTIAL DEVELOPMENT
LAKE GENEVA, WISCONSIN**

LEGEND

- XX AM Peak Hour Volumes (7:00-8:00 AM)
- (XX) PM Peak Hour Volumes (3:30-4:30PM)
- Negligible Traffic Volumes (Fewer than 3 vph)
- Proposed Driveway
- XXXXX 2012 Annual Average Daily Traffic (AADT)
- * 2009 Annual Average Daily Traffic (AADT)
- ** 2015 ADT (based on turning movement counts done as part of this study)

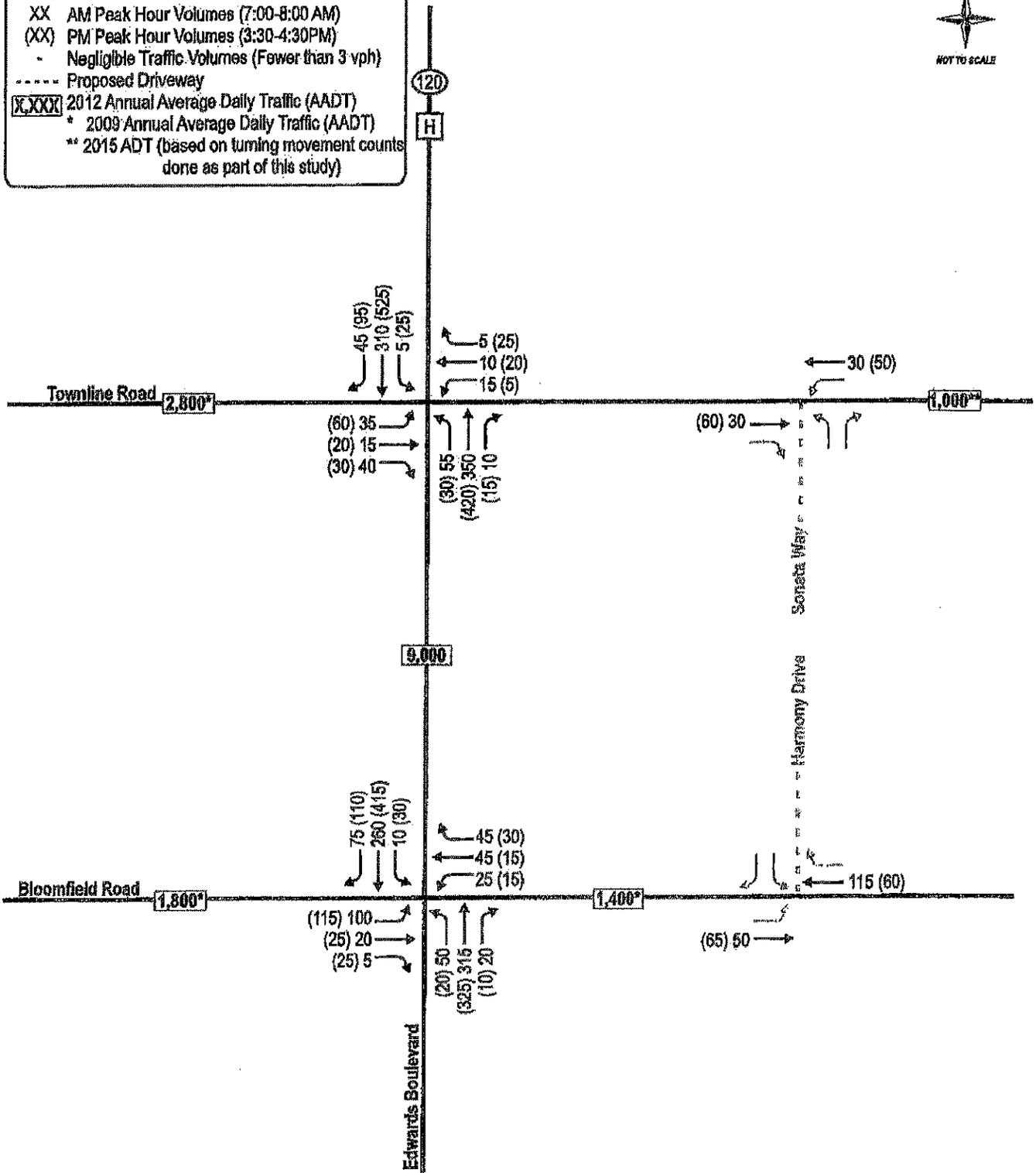


EXHIBIT 3-2
YEAR 2015 EXISTING TRAFFIC VOLUMES
SYMPHONY BAY RESIDENTIAL DEVELOPMENT
LAKE GENEVA, WISCONSIN

CHAPTER IV – DEVELOPMENT TRAFFIC

PART A – PROPOSED SITE TRAFFIC FORECASTING

To address any proposed future traffic impacts along the study area roadways and at the intersections adjacent to the development, it is necessary to identify the hourly and daily volume of traffic generated by the proposed developments within the study area. The traffic volumes expected to be generated by the proposed development are based on the size and type of the proposed uses, and on trip rates as published in the Institute of Transportation Engineer's (ITE) *Trip Generation Manual, 9th Edition, 2012*.

A1. Trip Generation

The expected trip generation for the Symphony Bay residential development is shown in Exhibit 4-1. As shown in Exhibit 4-1, the proposed development is expected to generate 225 new trips (85 entering/140 exiting) during a typical weekday morning peak hour and another 255 new trips (150 entering/105 exiting) during a typical weekday evening peak hour. On a typical weekday, the proposed development is expected to generate approximately 1,980 new trips (990 entering/990 exiting) under full build conditions.

A2. Trip Distribution

The trip distribution for the proposed residential development was determined based on existing traffic patterns within the study area, the type of proposed land uses, and anticipated growth areas located outside the study area. The expected trip distribution for the proposed residential development is shown in table format in Exhibit 4-1 and summarized as follows:

- 45-percent to/from the north on Edwards Boulevard;
- 25-percent to/from the south on Edwards Boulevard;
- 5-percent to/from the east on Townline Road;
- 10-percent to/from the west on Townline Road;
- 5-percent to/from the east on Bloomfield Road; and
- 10-percent to/from the west on Bloomfield Road.

A3. Trip Assignment

The peak hour new trips expected to be generated by the full build-out of the proposed Symphony Bay development were assigned to the study area roadways based on the above trip distribution and are shown on Exhibit 4-2.

PART B – BUILD (WITH DEVELOPMENT) TRAFFIC

The proposed Symphony Bay build traffic volumes assume the full build out of the site. The Year 2016 Build (with development) traffic was determined by adding the existing traffic volumes (Exhibit 3-2) to the Symphony Bay new trips (Exhibit 4-2). The Year 2016 Build (with development) traffic volumes are shown in Exhibit 4-3.

PART C – DEVELOPMENT LEVEL OF SERVICE

All movements at the study area intersections are anticipated to operate at LOS B or better conditions (LOS A along the mainline roadways of Bloomfield Road and Townline Road) under the Year 2016 Build (with development) traffic conditions with the following improvements at the study area intersections.

Bloomfield Road & proposed Harmony Drive

- Construct residential street with one entrance lane and one exit lane;
- Provide stop sign control on north approach only.

Townline Road & proposed Sonata Way

- Construct residential street with one entrance lane and one exit lane;
- Provide stop sign control on south approach only.

As shown in Exhibit 4-4, all movements at the study area intersections are anticipated to operate at LOS B or better conditions under the Year 2016 Build (with development) traffic conditions with the recommended improvements.

PART D – QUEUEING ANALYSIS

To estimate storage length requirements for turn bays at the study area intersections with improvements, a queuing analysis has been conducted. The Year 2016 Build (with development) Traffic expected maximum queue lengths are shown on Exhibit 4-5. Note that the 95th percentile probable queue lengths are typically used for the design of turn bay storage at stop sign controlled intersections. However, since the queue lengths at the study area intersections are expected to be minimal, no turn lanes are recommended as part of the development.

**Exhibit 4-1
On-Site Trip Generation Table**

Land Use	ITE Code	Proposed Size	Weekday Daily	AM Peak			PM Peak		
				In	Out	Total	In	Out	Total
Senior Adult Housing - Detached (Single Family House)	261	374 Units	1,380 (3.68)	60 (35%)	115 (65%)	175 (0.47)	120 (61%)	80 (39%)	200 (0.53)
Senior Adult Housing - Detached (Town house)	251	58 Units	210 (3.68)	10 (35%)	15 (65%)	25 (0.47)	20 (61%)	10 (39%)	30 (0.53)
Continuing Care Retirement Community	255	100 Units	240 (2.40)	10 (65%)	5 (35%)	15 (0.14)	5 (39%)	10 (61%)	15 (0.16)
Church	560	17,000 x 1,000 SF	150 (9.11)	5 (62%)	5 (38%)	10 (0.56)	5 (48%)	5 (52%)	10 (0.55)
Total New Trips			1,980	85	140	225	150	105	255

Notes: The new church will accommodate 250 seats; however, no peak hour trip rates are given in ITE based on number of seats. Therefore, since the daily trips for a 17,000-sf church equates to the same trips as a 250 seat church, the hours peak rates for the 17,000-sf church were used to determine the daily peak hour new trips.

TRIP DISTRIBUTION (New Trips)

North on Edwards Blvd	45%	890	40	85	70	50
South on Edwards Blvd	25%	500	15	35	40	25
East on Townline Road	5%	95	5	5	5	5
West on Townline Road	10%	200	10	15	15	10
East on Bloomfield Road	5%	95	5	5	5	5
West on Bloomfield Road	10%	200	10	15	15	10
	100%	1980	85	140	150	105

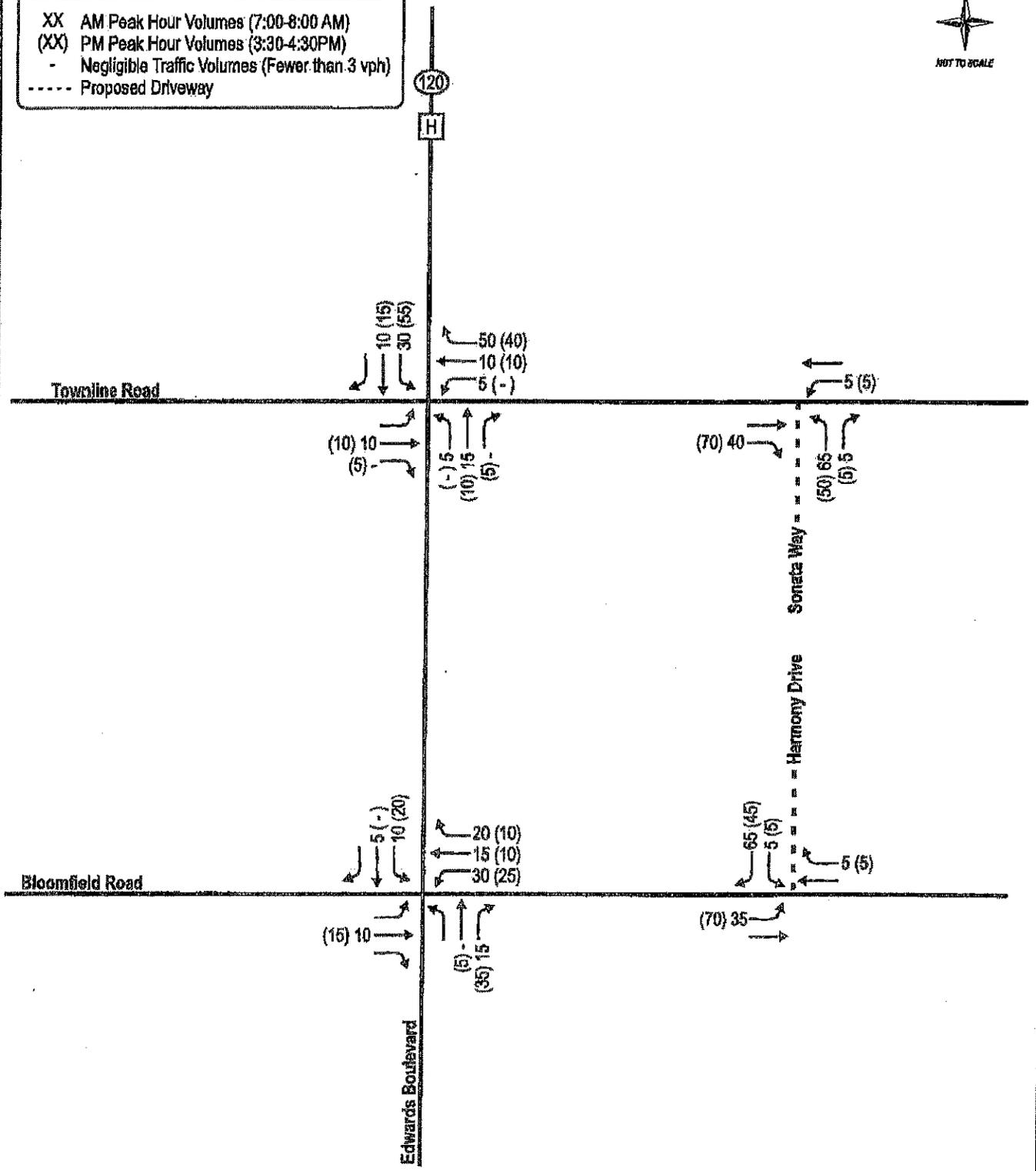


**EXHIBIT 4-1
ON-SITE TRIP GENERATION TABLE**

**SYMPHONY BAY RESIDENTIAL DEVELOPMENT
LAKE GENEVA, WISCONSIN**

LEGEND

- XX AM Peak Hour Volumes (7:00-8:00 AM)
- (XX) PM Peak Hour Volumes (3:30-4:30PM)
- Negligible Traffic Volumes (Fewer than 3 vph)
- Proposed Driveway



**EXHIBIT 4-2
DEVELOPMENT NEW TRIPS**

**SYMPHONY BAY RESIDENTIAL DEVELOPMENT
LAKE GENEVA, WISCONSIN**

LEGEND

- XX AM Peak Hour Volumes (7:00-8:00 AM)
- (XX) PM Peak Hour Volumes (3:30-4:30PM)
- Negligible Traffic Volumes (Fewer than 3 vph)
- Proposed Driveway

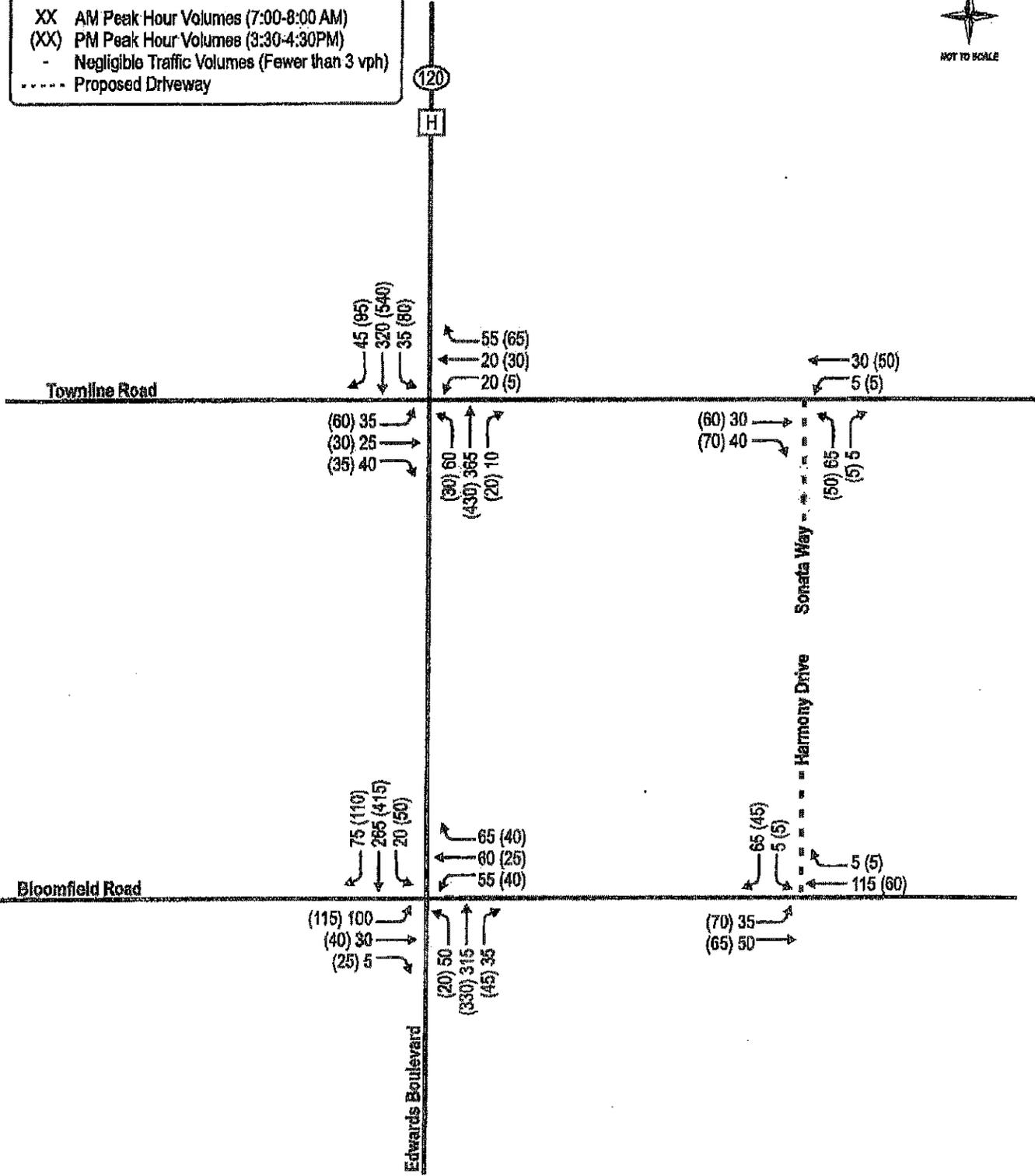


EXHIBIT 4-3
YEAR 2016 FULL BUILD TRAFFIC VOLUMES
SYMPHONY BAY RESIDENTIAL DEVELOPMENT
LAKE GENEVA, WISCONSIN

Exhibit 4-4
 Year 2016 Build Traffic Peak Hour Operating Conditions
 Recommended Geometrics and Traffic Control

Intersection	Traffic Control	Peak Hour	Level of Service per Movement by Approach											
			Eastbound			Westbound			Northbound			Southbound		
			LT	TH	RT	LT	TH	RT	LT	TH	RT	LT	TH	RT
Bloomfield Road & Proposed Harmony Drive	One-Way	AM	A	A	-	-	A	A	-	-	-	B	-	B
	Stop Sign	PM	A	A	-	-	A	A	-	-	-	B	-	B
Townline Road & Proposed Sonata Way	One-Way	AM	-	A	A	A	A	-	B	-	B	-	-	-
	Stop Sign	PM	-	A	A	A	A	-	B	-	B	-	-	-

Notes: (-) indicates a movement that is not possible or is prohibited.



EXHIBIT 4-4
 PEAK HOUR OPERATING CONDITIONS
 YEAR 2016 FULL BUILD TRAFFIC WITH IMPROVEMENTS
 SYMPHONY BAY RESIDENTIAL DEVELOPMENT
 LAKE GENEVA, WISCONSIN

LEGEND

- Stop Sign
- Existing Lane Configuration
- Proposed Lane Configuration
- Proposed Driveway
- XX' AM Peak Hour Queue (in Feet)
- (XX') PM Peak Hour Queue (in Feet)



120

H

Townline Road

Bloomfield Road

Edwards Boulevard

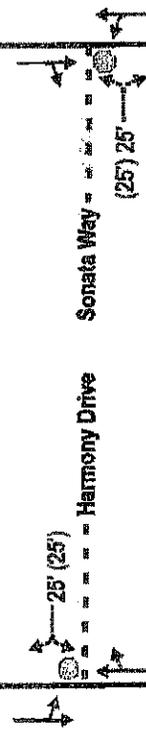


EXHIBIT 4-5

MAXIMUM EXPECTED QUEUE LENGTHS
YEAR 2016 FULL BUILD TRAFFIC WITH IMPROVEMENTS
SYMPHONY BAY RESIDENTIAL DEVELOPMENT
LAKE GENEVA, WISCONSIN



CHAPTER V – RECOMMENDATIONS AND CONCLUSION

PART A – RECOMMENDATIONS

A1. Existing Traffic – Recommended Improvements

The study area intersections were analyzed based on the procedures set forth in the *2010 Highway Capacity Manual (HCM)*. Intersection operation is defined by “level of service”. Level of Service (LOS) is a quantitative measure that refers to the overall quality of flow at an intersection ranging from very good, represented by LOS ‘A’, to very poor, represented by LOS ‘F’. For the purpose of this study, LOS D or better was used to define desirable peak hour operating conditions.

A2. Year 2016 Build (With development) Traffic – Recommended Improvements

Year 2016 Build (with development) traffic volumes include the full build out of the Symphony Bay development. Based on the analysis completed as part of this study, the following improvements are recommended to accommodate the Year 2016 Build (with development) traffic volumes.

Bloomfield Road & proposed Harmony Drive

- Construct residential street with one entrance lane and one exit lane;
- Provide stop sign control on north approach only.

Townline Road & proposed Sonata Way

- Construct residential street with one entrance lane and one exit lane;
- Provide stop sign control on south approach only.

It is noted that the construction of an eastbound bypass lane on Bloomfield Road and a westbound bypass lane on Townline Road were investigated at the two development access roadways and it was determined that a bypass lane was not needed from an operations (LOS) perspective since all movements are expected to operate at LOS B or better under the year 2016 Build conditions. In addition, based on by-pass guidelines utilized in other neighboring counties, since the daily volumes on the side road are not expected to exceed 2,500-vehicles per day, a bypass lane is not expected to be necessary at the two development access roadways.

PART B – CONCLUSION

All movements at the study area intersections are expected to operate safely and efficiently through the construction year 2016 with the recommended improvements under full build conditions.